



AN AMERICAN BUILDING

“Building Success - Through Partnerships”

A.B.C. JOB NO. 101-8746-01 Sealed
Finals

1. AMERICAN BUILDINGS COMPANY ASSUMES NO RESPONSIBILITY OR LIABILITY FOR FOUNDATION, FLOOR OR SLAB DESIGN OR CONSTRUCTION.
2. THE FOUNDATION DESIGN SHOULD BE DONE WITH DUE REGARD TO THE SPECIFIC SOIL CONDITIONS PRESENT AT THE ACTUAL JOBSITE.
3. FOUNDATION MUST BE DESIGNED FOR THE APPLICABLE REACTIONS AS THEY APPLY TO A PARTICULAR BUILDING AND MUST BE ADEQUATE TO RESIST ALL OF THE CRITICAL COMBINATIONS FOR EACH OF THE VARIOUS LOADING CONDITIONS. THESE REACTIONS AND LOAD COMBINATIONS MUST BE USED TO DETERMINE THE DESIGN LOADS TO BE RESISTED BY THE FOUNDATIONS.
4. REINFORCING BARS, WIRE MESH, ANCHOR BOLT SHEAR ANGLES, THE RODS AND / OR HAIRPINS (HOOK BARS) SHOULD BE INCORPORATED AS REQUIRED INTO THE FOUNDATION DESIGN. THE HORIZONTAL THRUST AT THE COLUMN BASE ACTING IN CONJUNCTION WITH APPLICABLE VERTICAL REACTIONS, MUST BE SUSTAINED BY HAIRPINS, TIE RODS, BUTTRESSES, OR OTHER DEPENDABLE MEANS.
5. COLUMN FOOTING SHOULD EXTEND A MINIMUM OF 12 INCHES INTO NATURAL SOIL, OR WHERE FILL IS USED, THE FILL MUST BE PROPERLY COMPACTED OR THE FOOTING SHALL EXTEND TO THE NATURAL GRADE. IN ALL CASES THE FOOTING SHALL EXTEND AT LEAST 6 INCHES BELOW THE LOCAL FROST LINE.
6. EXPANSION OR CONSTRUCTION JOINTS SHALL BE LOCATED AS REQUIRED IN FOUNDATION WALLS AND SLAB.
7. THE TOP OF THE FOUNDATION OR FLOOR SHALL BE SQUARE, LEVEL AND SMOOTH. ANCHOR BOLTS SHALL BE ACCURATELY SET TO A TOLERANCE $\pm 1/16$ INCH ON DIMENSIONS WITHIN THE GROUP SPACING FOR AN INDIVIDUAL COLUMN. ALL OTHER DIMENSIONS SHALL HAVE A $\pm 1/8$ INCH TOLERANCE.
8. COLUMN BASE PLATES ARE DESIGNED NOT TO EXCEED THE ALLOWABLE BEARING STRESS OF CONCRETE THAT HAS A MINIMUM COMPRESSIVE STRENGTH OF 2500 P.S.I. AT 28 DAYS.
9. UNLESS EXPLICITLY NOTED OTHERWISE, ALL EMBEDDED STRUCTURAL STEEL (INCLUDING ANCHOR BOLTS), OTHER MATERIALS, AND LABOR SHALL BE SUPPLIED BY THE FOUNDATION CONTRACTOR.
10. ANCHOR BOLTS SHOULD BE AS SHOWN AND CALLED FOR, INCLUDING PROJECTION FROM CONCRETE, DIAMETER AND QUANTITY.
11. THE EMBEDMENT OF THE ANCHOR BOLTS IN THE CONCRETE AND CONFIRMING ADEQUACY OF ANCHOR BOLT EDGE DISTANCE IS THE RESPONSIBILITY OF THE FOUNDATION DESIGNER. THE FRAME REACTIONS ARE CONSIDERED THE MINIMUM LOADS TO BE DEVELOPED.
12. ALL ANCHOR BOLTS SHALL BE ASTM A307 OR EQUAL IN ORDER TO CONFORM TO A.B.C. DESIGN ASSUMPTIONS BASED ON THE ALLOWABLE STRESSES GIVEN IN THE AISC MANUAL OF STEEL CONSTRUCTION.
13. ALL ANCHOR BOLTS SHALL HAVE A 3 INCH PROJECTION (UNLESS NOTED) ABOVE THE CONCRETE BEARING SURFACE, EXCEPT 1/2 INCH DIAMETER BOLTS LOCATED AT DOORS WHICH SHALL HAVE A PROJECTION OF 1 INCH. ALL BOLTS SHALL HAVE A MINIMUM THREAD LENGTH 1/4 INCH LESS THAN THE PROJECTION. PROJECTING THREADS SHOULD BE GREASED OR OTHERWISE PROTECTED PRIOR TO COLUMN ERECTION.
14. ANCHOR BOLT DIAMETERS FOR THE PRIMARY FRAMING AND ENDWALL FRAMING ARE DENOTED AT RESPECTIVE BASE PLATE DETAILS OR ON THE BOLT PLACEMENT PLAN. ANCHOR BOLTS FOR FRAMED OPENINGS SHALL BE 1/2 INCH DIAMETER UNLESS OTHERWISE NOTED.



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|--|--|--|--|------|------|------|------|------|---|--|--|--|--|------|------|----------|--------|---------------------|--------------------------|--|--|--|-----------------|-------------|-----------|------|-----------------------------|----------|--|--|--|--|--|--|---|--|---|--|
| 10. 24 GA. LOG-SEAM MODIFIED WITH 3M-VHB TAPE #4950 MEETS CORPS OF ENGINEERS GUIDE SPECIFICATION 07416. | | | | | | | | | | 11. 24 GA. LOG-SEAM MODIFIED WITH 3M-VHB TAPE #4950 MEETS CORPS OF ENGINEERS GUIDE SPECIFICATION 07416. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| REVISIONS | | | | | MADE | CK'D | DATE | ENGR | DATE | REVISIONS | | | | | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | | | | | SCALE: | | NONE | | DATE | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | | | <div>EL PASO ILLINOIS</div> <div>AMERICAN BUILDINGS COMPANY</div> | | DRAWING NUMBER: 61874601 ABC-1 REV.NO. 2 | |
| | | | | | | | | | 1 | UPDATED PER CUSTOMER CHANGES | | | | | BJC | SB | 08/26/08 | DBRYAN | 08/26/08 | [*] FOR CONSTRUCTION | | | | | DRAWN BY: | | JAW | | 06/25/08 | | | | | | | | | | |
| | | | | | | | | | 2 | UPDATED FOR CONSTRUCTION | | | | | BJC | SB | 08/27/08 | DBRYAN | 08/27/08 | [] FOR APPROVAL | | | | | CHECKED BY: | | AL | | 06/25/08 | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | [] FOR PERMIT ONLY | | | | | DESIGN APPD BY: | | DBRYAN | | 06/25/08 | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | [] FOR PRELIMINARY | | | | | CAD BY: | | J. WILSON | | 06/25/08 09:39:53 Ver. 30.1 | | | | | | | | | | | |

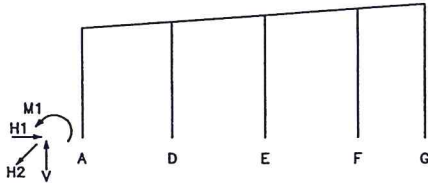
1. UNIFORM BUILDING CODE (IBCO) - # FA-285-CARSON CITY, NEVADA PLANT.
2. INDIANA - 3 MASTER PLAN FILE NUMBERS
CABLE BUILDINGS : M-295147
SINGLE SLOPE BUILDINGS : M-295140
LEAN-TO BUILDINGS : M-295144
3. WISCONSIN-#200802-M, 13 BASIC STYLES.
4. CITY OF CLEVELAND, OHIO DOCKETS S-52-82.
5. CANADIAN WELDING BUREAU DIVISION I CERTIFICATION AT EL PASO, CARSON CITY & LACROSSE.
6. AISC QUALITY CERTIFICATION, CATEGORY MB.
7. PLANT CERTIFICATION AT CARSON CITY FOR CITY OF SEATTLE AND FOR OGDEN, UTAH.
8. STANDING SEAM II / STANDING SEAM 360 WIND UPLIFT-CLASS 90 (UL 90) CONSTRUCTION NO.S 93 AND 210A AS LISTED IN UNDERWRITERS LABORATORIES ROOFING MATERIALS AND SYSTEM DIRECTORY.
9. LOC SEAM / LOC SEAM 360 WIND UPLIFT-CLASS 90 (UL 90) CONSTRUCTION NO.S 238, 238A, 238B, 238C, 331, 332, 336, 435, 451, 487 & 567 AS LISTED IN UNDERWRITERS LABORATORIES ROOFING MATERIALS AND SYSTEM DIRECTORY.
10. LONG SPAN / LONG SPAN III PANEL WIND UPLIFT-CLASS 90 (UL 90) CONSTRUCTION NO.S 71, 161 & 167 AS LISTED IN UNDERWRITERS LABORATORIES ROOFING MATERIALS AND SYSTEM DIRECTORY.
11. 22 GA. STANDING SEAM 360 HAS MET FACTORY MUTUAL STANDARD 4471 UP TO AND INCLUDING 1-120.
12. 24 GA. STANDING SEAM 360 HAS MET FACTORY MUTUAL STANDARD 4471 UP TO AND INCLUDING 1-120, CORPS OF ENGINEERS GUIDE SPECIFICATION 07416 AND HAS BEEN TESTED IN ACCORDANCE WITH ASTM E-1592.
13. 24 & 26 GA. LONGSPAN III HAS MET FACTORY MUTUAL STANDARD 4471 UP TO AND INCLUDING 1-150.
14. 22 GA. LOC-SEAM 360 HAS MET FACTORY MUTUAL STANDARD 4471 UP TO AND INCLUDING 1-180.
15. 24 GA. LOC-SEAM 360 HAS MET FACTORY MUTUAL STANDARD 4471 UP TO AND INCLUDING 1-120, CORPS OF ENGINEERS GUIDE SPECIFICATION 07416 AND HAS BEEN TESTED IN ACCORDANCE WITH ASTM E-1592.
16. 24 GA. LOC-SEAM MODIFIED WITH 3M-VHB TAPE #4950 MEETS CORPS OF ENGINEERS GUIDE SPECIFICATION 07416.

Δ D+C + L
 D+C + S
 D + W
 D+C + 0.70E
 D+C + $\frac{3}{4}L$ + $\frac{3}{4}W$
 D+C + $\frac{3}{4}S$ + $\frac{3}{4}W$
 D+C + $\frac{3}{4}L$ + $\frac{3}{4}(.7E)$
 D+C + $\frac{3}{4}S$ + $\frac{3}{4}(.7E)$
 .6D + W
 .6(D+C) + .7E

| SHEET NUMBER | DATE | REV. NO. | SHEET NUMBER | DATE | REV. NO. |
|-----------------|----------|----------|-----------------|------|----------|
| ABC-1 | 08/27/08 | 2 | SED-015 | | 0 |
| ABC-2 | 08/26/08 | 0 | SED-016 | | 0 |
| AB-1 | 06/25/08 | 0 | SED-017 | | 0 |
| AB-2 | 06/25/08 | 0 | SED-018 | | 0 |
| AB-3 | 08/26/08 | 1 | | | |
| E-01 | 08/27/08 | 0 | | | |
| E-02 | 08/27/08 | 0 | | | |
| E-03 | 08/27/08 | 0 | | | |
| E-04 | 08/27/08 | 0 | | | |
| E-05 | 08/27/08 | 0 | | | |
| E-06 | 08/27/08 | 0 | | | |
| E-07 | 08/27/08 | 0 | | | |
| E-08 | 08/27/08 | 0 | | | |
| E-09 | 08/27/08 | 0 | | | |
| E-10 | 08/27/08 | 0 | | | |
| E-11 | 08/27/08 | 0 | | | |
| SED-001 | | 0 | | | |
| SED-002 | | 0 | | | |
| SED-003 | | 0 | | | |
| SED-004 | | 0 | | | |
| SED-005 | | 0 | | | |
| SED-006 | | 0 | | | |
| SED-007 | | 0 | | | |
| SED-008 | | 0 | | | |
| SED-009 | | 0 | | | |
| SED-010 | | 0 | | | |
| SED-011 | | 0 | | | |
| SED-012 | | 0 | | | |
| SED-013 | | 0 | | | |
| SED-014 | | 0 | | | |

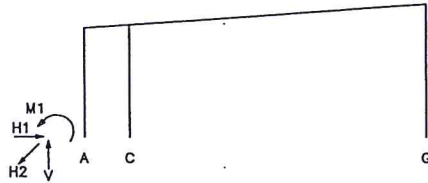
THE CERTIFYING ENGINEER HERewith IS NOT THE ENGINEER OF RECORD FOR THE OVERALL PROJECT AND IS ONLY CERTIFYING THAT THE DESIGN OF THE METAL BUILDING COMPONENTS FURNISHED BY AMERICAN BUILDINGS COMPANY SATISFY THE DESIGN REQUIREMENTS SPECIFIED ABOVE AND ON THE A.B.C. CONTRACT. THIS CERTIFICATION EXCLUDES THE ERECTION OF THE STRUCTURE.

REACTION SCHEMATICS



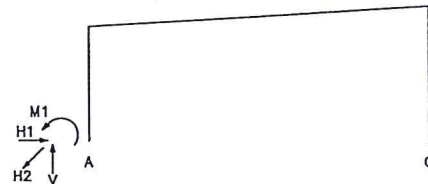
| LOADING CONDITION | FRAME LINE 1 | | | | | | | | | |
|-------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|
| | B | | | D | | | E | | | G |
| | V (kips) | V (kips) | H2 (kips) | V (kips) | H1 (kips) | H2 (kips) | V (kips) | H1 (kips) | H2 (kips) | V (kips) |
| D | +0.3 | +0.8 | | +0.7 | | | +0.7 | | | +0.2 |
| C | +0.5 | +1.0 | | +0.9 | | | +0.8 | | | +0.4 |
| L | -0.1 to +1.7 | -0.2 to +5.0 | | +5.0 | | | -0.2 to +4.2 | | | -0.2 to +1.3 |
| S | +1.6 | +4.7 | | +4.1 | | | +4.0 | | | +1.1 |
| W | -1.2 | -3.7 | -2.5 to +2.2 | -3.3 to +2.3 | -1.7 to +1.7 | -2.6 to +2.3 | -3.2 to +2.3 | -1.7 to +1.7 | -2.4 to +2.2 | -0.8 |
| E | | | -0.8 to +0.8 | -0.6 to +0.6 | | | -0.8 to +0.8 | -0.6 to +0.6 | | |

H1 = horizontal force in the plane of frame
H2 = horizontal force perpendicular to the plane of frame



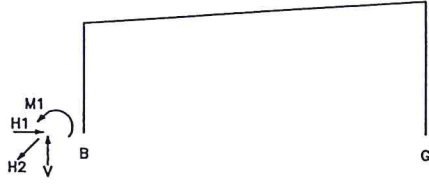
| LOADING CONDITION | CURTAIN WALL AT FRAME LINE 4 | | |
|-------------------|------------------------------|--------------------|--------------------|
| | A | | C |
| | V (kips) | H2 (kips) | H2 (kips) |
| W | -4.7 | -4.5 to +4.5 | -1.2 to +1.3 |
| E | -0.8 | -0.8 to +0.8 | |

H1 = horizontal force in the plane of frame
H2 = horizontal force perpendicular to the plane of frame



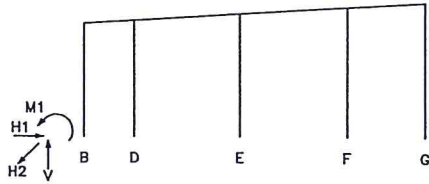
| LOADING CONDITION | FRAME LINE 5 | | | | | |
|----------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|
| | A | | | G | | |
| | V (kips) | H1 (kips) | H2 (kips) | V (kips) | H1 (kips) | H2 (kips) |
| D | +3.1 | +1.2 | | +3.2 | -1.2 | |
| C | +2.1 | +1.0 | | +2.1 | -1.0 | |
| L | +8.4 | +3.8 | | +8.4 | -3.8 | |
| S | +14.0 | +6.4 | | +14.0 | -6.4 | |
| W | -8.1 to +1.0 | -4.7 to +1.4 | | -7.6 to +1.2 | -1.6 to +4.0 | |
| E | -0.1 to +0.1 | -0.2 to +0.2 | | -0.1 to +0.1 | -0.2 to +0.2 | |
| E (at X braced bays) | -0.4 to +0.4 | -0.2 to +0.2 | -0.3 to +0.3 | -0.4 to +0.4 | -0.2 to +0.2 | -0.3 to +0.3 |

H1 = horizontal force in the plane of frame
H2 = horizontal force perpendicular to the plane of frame



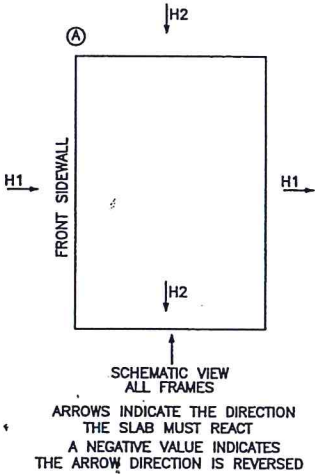
| LOADING CONDITION | FRAME LINES 2-4 | | | | | |
|----------------------|---------------------|--------------------|--------------------|---------------------|--------------------|--------------------|
| | A | | | G | | |
| | V (kips) | H1 (kips) | H2 (kips) | V (kips) | H1 (kips) | H2 (kips) |
| D | +3.5 | +1.7 | | +3.7 | -1.7 | |
| C | +2.2 | +1.3 | | +2.3 | -1.3 | |
| L | +9.0 | +5.2 | | +9.0 | -5.2 | |
| S | +15.0 | +8.7 | | +15.0 | -8.7 | |
| W | -8.5 to +1.0 | -5.8 to +1.6 | | -8.1 to +1.0 | -1.4 to +4.7 | |
| W (at X braced bays) | -12.6 to +1.0 | -5.8 to +1.6 | -4.5 to +4.5 | -14.3 to +1.0 | -1.4 to +4.7 | -4.8 to +4.8 |
| E | -0.1 to +0.1 | -0.3 to +0.3 | | -0.1 to +0.1 | -0.2 to +0.2 | |
| E (at X braced bays) | -0.5 to +0.5 | -0.3 to +0.3 | -0.5 to +0.5 | -0.6 to +0.6 | -0.2 to +0.2 | -0.5 to +0.5 |

H1 = horizontal force in the plane of frame
H2 = horizontal force perpendicular to the plane of frame



| LOADING CONDITION | FRAME LINE 6 | | | | | | | | | |
|-------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|-----------|--------------------|--------------------|
| | B | | | D | | | E | | | G |
| | V (kips) | V (kips) | H1 (kips) | V (kips) | H1 (kips) | H2 (kips) | V (kips) | H1 (kips) | H2 (kips) | V (kips) |
| D | +0.1 | +0.7 | | | | | +0.9 | | | +0.8 |
| C | +0.1 | +0.6 | | | | | +0.8 | | | +0.7 |
| L | -0.5 to +1.2 | -0.4 to +4.5 | | | | | +6.1 | | | -0.4 to +5.2 |
| S | +0.7 | +4.1 | | | | | +5.6 | | | +4.8 |
| W | -0.7 | -4.2 to +2.3 | -1.7 to +1.7 | -1.7 to +1.9 | -5.8 to +2.3 | -1.7 to +1.7 | -2.4 to +2.6 | -5.0 | -2.2 to +2.4 | -1.4 |
| E | | -0.8 to +0.8 | -0.6 to +0.6 | | -0.8 to +0.8 | -0.6 to +0.6 | | | | |

H1 = horizontal force in the plane of frame
H2 = horizontal force perpendicular to the plane of frame

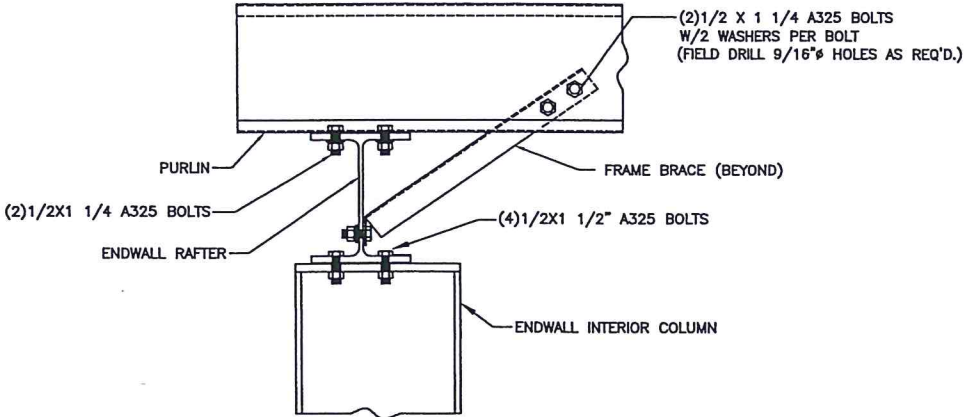


SCHEMATIC VIEW
ALL FRAMES

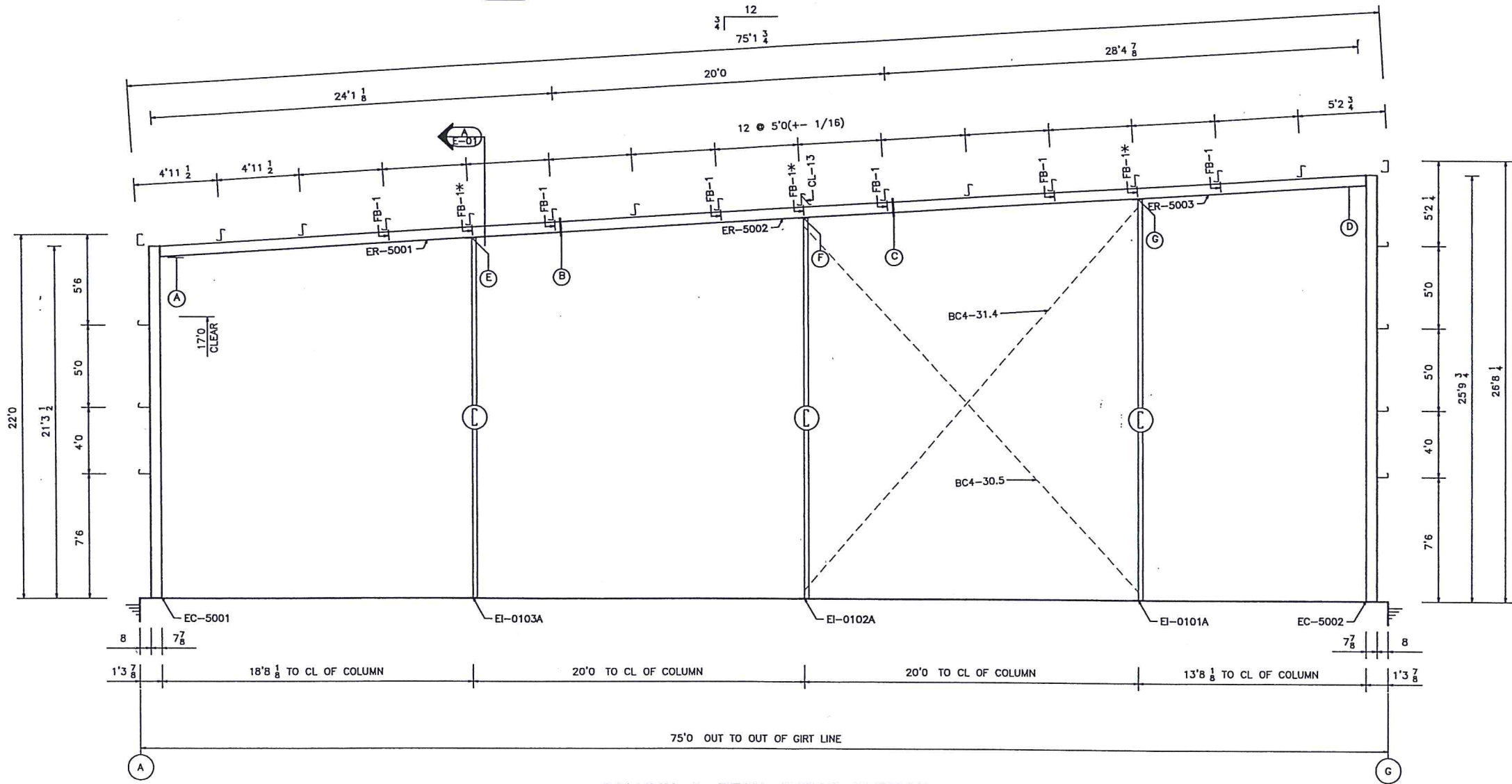
ARROWS INDICATE THE DIRECTION
THE SLAB MUST REACT
A NEGATIVE VALUE INDICATES
THE ARROW DIRECTION IS REVERSED

| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | SCALE: | NONE | DATE | CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
|-----|-----------|------|------|------|------|------|-----|------------------------------|------|------|----------|------|---------|--|---------------|----------|-----------|--|----------------------------|----------|
| | | | | | | | 1 | UPDATED PER CUSTOMER CHANGES | BJC | DB | 08/26/08 | DB | 8/26/08 | [*] FOR CONSTRUCTION [] FOR APPROVAL [] FOR PERMIT ONLY [] FOR PRELIMINARY | DRAWN BY: JAW | | 06/25/08 | 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | AMERICAN BUILDINGS COMPANY | |
| | | | | | | | | | | | | | | CAD BY: J. WILSON | 08/18/08 | 14:29:16 | Ver. 30.1 | DRAWING NUMBER: 61874601 AB-3 | REV.NO. 1 | |


| SPlice BOLT TABLE | | | | |
|-------------------|-----|-------------|-------|-----------------|
| SPLICE | QTY | SIZE | DEPTH | CLEAR AT SPLICE |
| A | 4 | 1/2 X 1 1/4 | 7 7/8 | 20'7 1/8 |
| B | 8 | 3/4 X 2 | 7 7/8 | 21'11 |
| C | 8 | 3/4 X 2 | 7 7/8 | 23'2 |
| D | 4 | 1/2 X 1 1/4 | 7 7/8 | 25'1 5/16 |
| E | 4 | 1/2 X 1 1/2 | 1'0 | 21'9 9/16 |
| F | 4 | 1/2 X 1 1/2 | 1'0 | 23'0 9/16 |
| G | 4 | 1/2 X 1 1/2 | 1'0 | 24'3 9/16 |



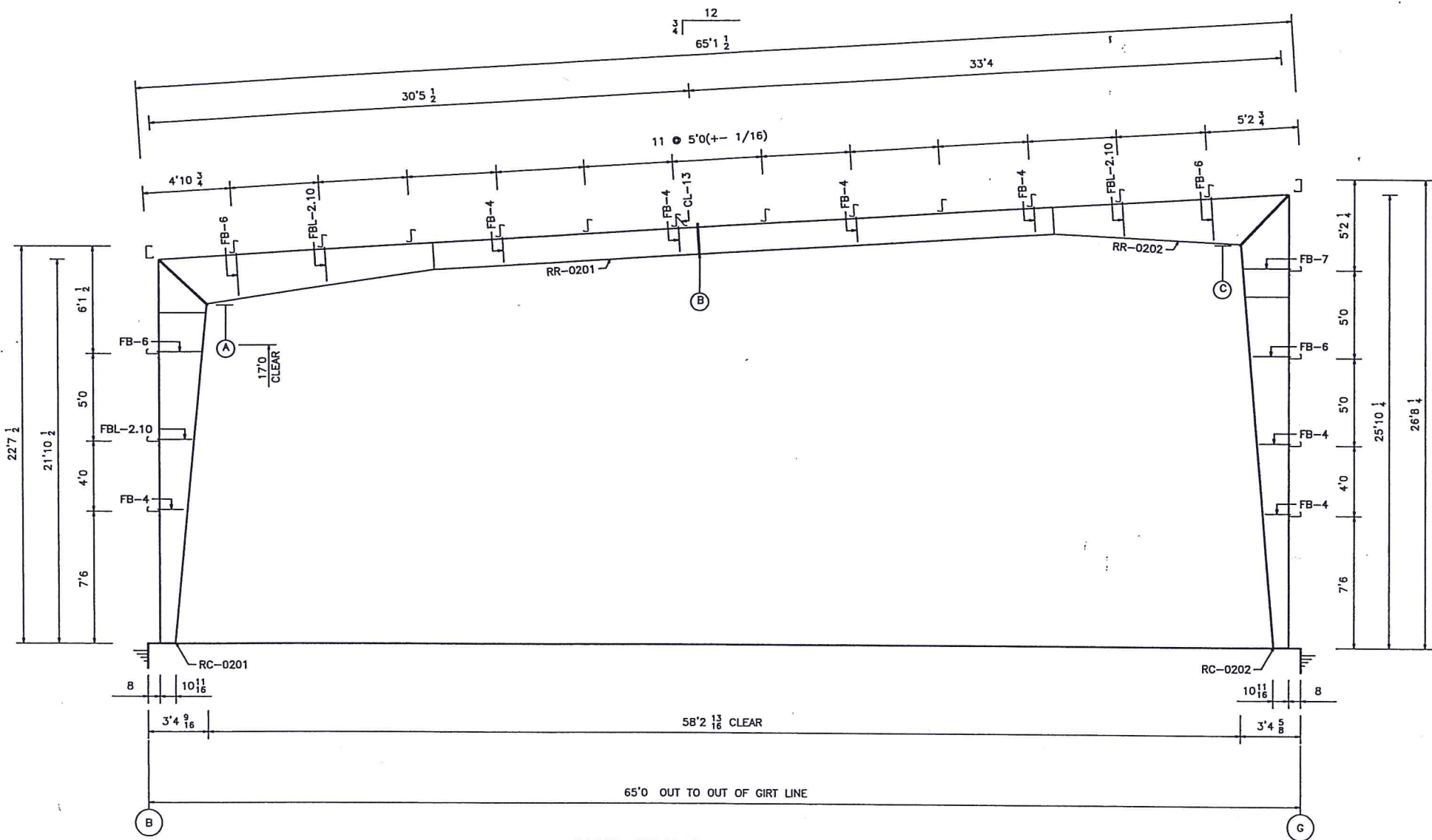
SECTION A
E-01
*AT NOTED LOCATIONS




COLUMN & BEAM CROSS SECTION
FRAME LINE 1

| REVISIONS | | | | | | REVISIONS | | | | | | DRAWING SUBMITTAL STATUS | | | SCALE: NONE | | DATE | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | EL PASO ILLINOIS | |
|-----------|--|--|--|--|--|-----------|--|--|--|--|--|--------------------------|--|--|-------------|--|------|---|--|----------------------------|--|
| | | | | | | | | | | | | | | | | | |  | | AMERICAN BUILDINGS COMPANY | |
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| SPlice Bolt Table | | | | |
|-------------------|-----|---------|-----------|-----------------|
| SPlice | QTY | SIZE | DEPTH | CLEAR AT SPlice |
| A | 10 | 3/4 X 2 | 2'8 9/16 | 19'3 3/8 |
| B | 10 | 3/4 X 2 | 1'6 11/16 | 22'0 1/4 |
| C | 10 | 3/4 X 2 | 2'8 5/8 | 22'10 7/8 |

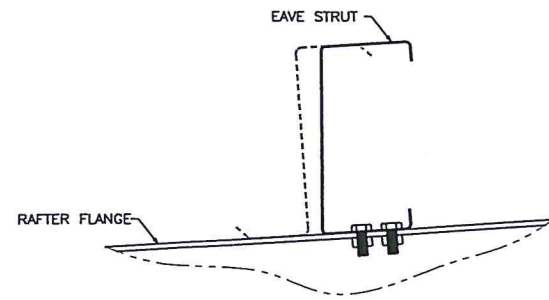


RIGID FRAME CROSS SECTION
 FRAME ID: 61874601.02A
 FRAME LINE 5

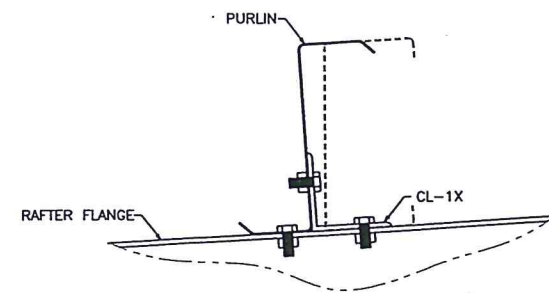
| REVISIONS | | | | | | REVISIONS | | | | | | DRAWING SUBMITTAL STATUS | | | | | | SCALE: | | | DATE | | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | EL PASO ILLINOIS  AMERICAN BUILDINGS COMPANY | | DRAWING NUMBER: 61874601 E-03 | | REV.NO. 0 | |
|-----------|------|------|------|------|------|-----------|------|------|------|------|------|--|-----------------|------|-----------------------------|--|--|--------|--|--|------|--|--|--|--|--|--|--|----------------------------------|--|--------------|--|
| NO. | MADE | CK'D | DATE | ENGR | DATE | NO. | MADE | CK'D | DATE | ENGR | DATE | [*] FOR CONSTRUCTION | DRAWN BY: | BJC | 08/27/08 | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | [] FOR APPROVAL <td>CHECKED BY:</td> <td>SB</td> <td>9/3/08</td> | CHECKED BY: | SB | 9/3/08 | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | [] FOR PERMIT ONLY <td>DESIGN APPD BY:</td> <td>Daga</td> <td>4/3/08</td> | DESIGN APPD BY: | Daga | 4/3/08 | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | [] FOR PRELIMINARY <td>CAD BY:</td> <td>BJC</td> <td>08/22/08 14:09:58 Ver. 30.1</td> | CAD BY: | BJC | 08/22/08 14:09:58 Ver. 30.1 | | | | | | | | | | | | | | | | | |

NOTE: USE CL-15 CLIPS AT ALL
PURLINS AT FRAME LINE 6

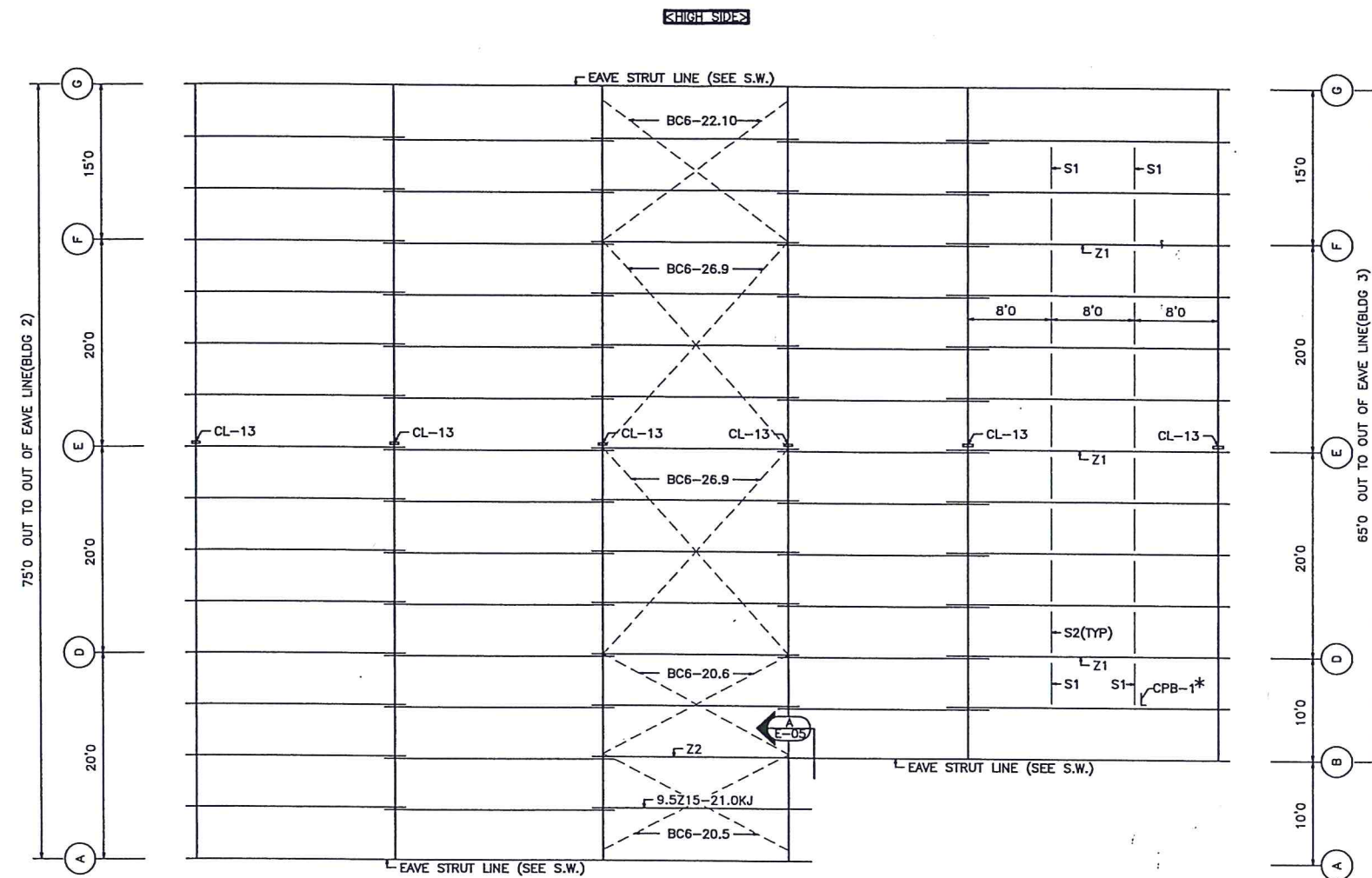
PURLIN LAPS = 2'-0" TYPICAL U.N.
PURLIN LAPS = 4'-0" (FL 5 ONLY)



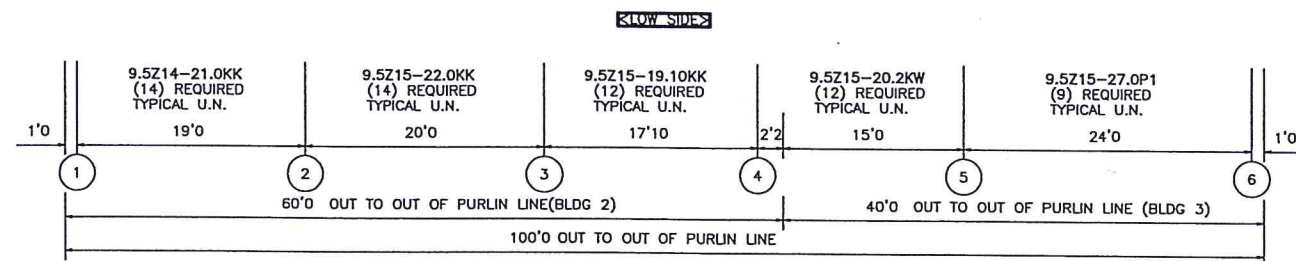
SECTION A(NEAR)
E-05
(1/2" X 1 1/4" A325 BOLTS TYP)



SECTION A(FAR)
E-05
(1/2" X 1 1/4" A325 BOLTS TYP)



* NOTE:
CPB-1 CLIP TYPICAL EACH END OF "CEE" BRIDGING



S1 - 6C16-4.11 1/4NN
S2 - CBA450
Z1 - 9.5Z14-27.0P1
Z2 - 9.5Z15-18.9 3/4KZ

ROOF FRAMING PLAN

| REVISIONS | | | | | | | REVISIONS | | | | | | | DRAWING SUBMITTAL STATUS | | SCALE: | | NONE | | DATE | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | | EL PASO | | ILLINOIS | | | | |
|-----------|--|--|--|--|--|--|-----------|--|--|--|--|--|--|--------------------------|--|--------|--|------|--|------|--|--|--|--|--|---------|--|----------|--|--|--|--|
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* **NOTE:**
USE RCS CLAMPS @ SPECIFIED LOCATIONS
(SEE RC01AEA IN SED'S)



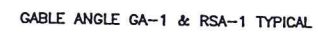
| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE |
|-----|-----------|------|------|------|------|------|-----|-----------|------|------|------|------|------|
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| DRAWING SUBMITTAL STATUS | | SCALE: | NONE | DATE |
| <input type="checkbox"/> FOR CONSTRUCTION <input type="checkbox"/> FOR APPROVAL <input type="checkbox"/> FOR PERMIT ONLY <input type="checkbox"/> FOR PRELIMINARY | | DRAWN BY: | BJC | 08/27/08 |
| | | CHECKED BY: | SB | 9/3/08 |
| | | DESIGN APPD BY: | Dmya | 9/4/08 |
| CAD BY: | | BJC | 08/25/08 | 14:43:23 |
| | | | | Ver. 30.1 |

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| CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | |
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|--|---------------|--------------|
| EL PASO  AMERICAN BUILDINGS COMPANY | | ILLINOIS |
| DRAWING NUMBER | 61874601 E-06 | REV.NO. 0 |

GIRT LAPS = 2'-0" TYPICAL U.N.

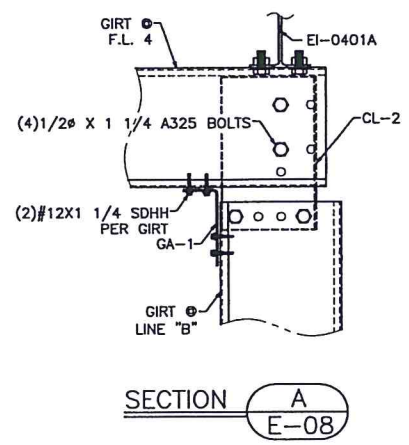
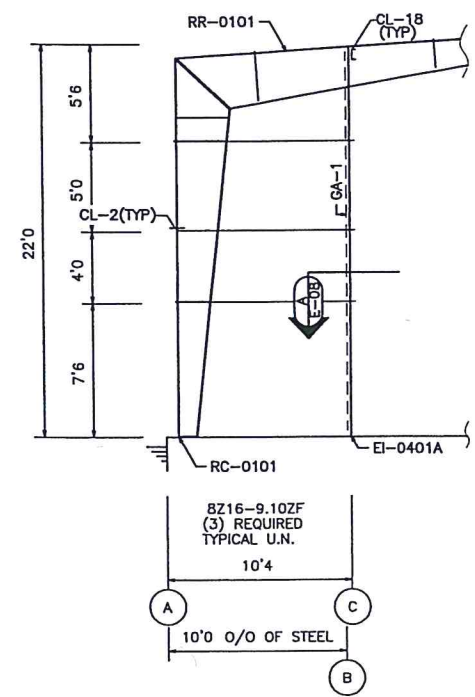


GABLE ANGLE GA-1 & RSA-1 TYPICAL

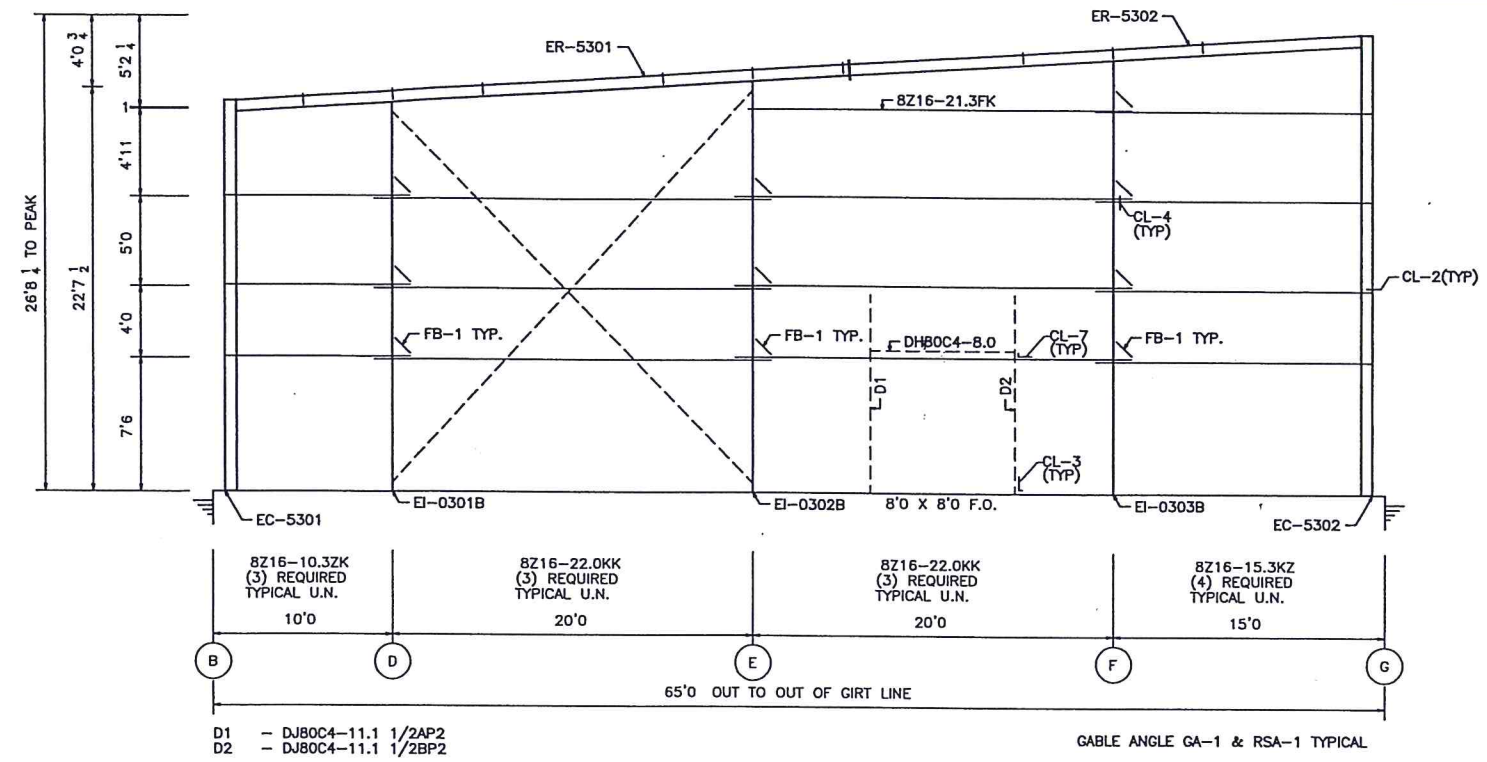


| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | SCALE: | NONE | DATE | CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
|-----|-----------|------|------|------|------|------|-----|-----------|------|------|------|------|------|--------------------------|---------------------|------|-----------------------------|----------------------|---------|----------|
| | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | DRAWN BY: BJC | | 08/27/08 | 3129 MAIN | | |
| | | | | | | | | | | | | | | [] FOR APPROVAL | CHECKED BY: SB | | 9/2/08 | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | [] FOR PERMIT ONLY | DESIGN APPD BY: Dmy | | 9/3/08 | HORIZON CONSTRUCTION | | |
| | | | | | | | | | | | | | | [] FOR PRELIMINARY | | | | | | |
| | | | | | | | | | | | | | | CAD BY: BJC | | | 08/25/08 15:14:52 Ver. 30.1 | | | |

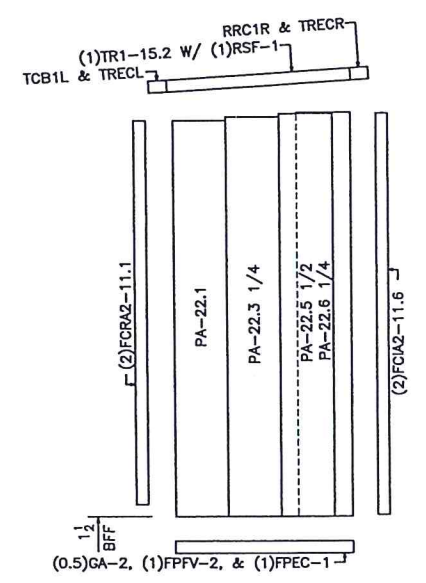
THIS ENDWALL FRAME MUST NOT BE ERECTED WITHOUT THE CROSS BRACING. REFER TO THE FRAME CROSS SECTION AT THIS FRAME LINE FOR THE REQUIRED CROSS BRACING.



ENDWALL FRAMING AT FRAME LINE 4
GABLE ANGLE GA-1 & RSA-1 TYPICAL

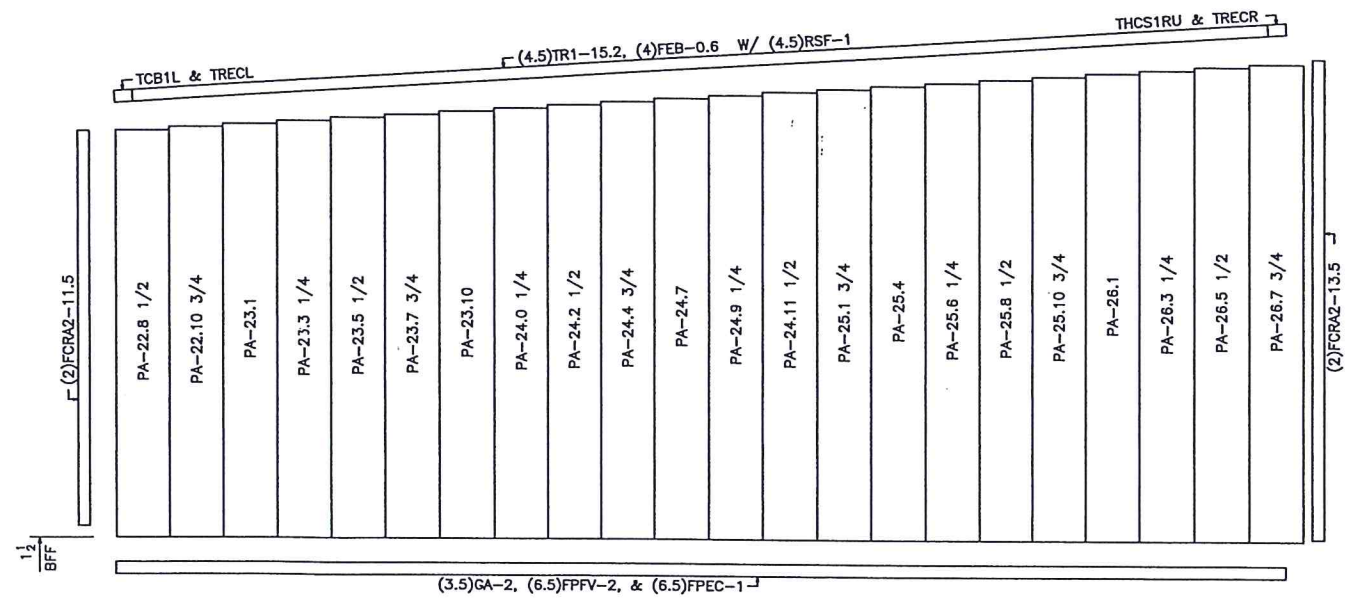


ENDWALL FRAMING AT FRAME LINE 6
GIRT LAPS = 2'-0" TYPICAL U.N.



NOTE: USE (2) 3/4" x 1 3/4" A325 BOLTS W/ WASHERS @ CL-18 TO E/W RAFTER CONNECTION.
USE (2) 3/4" x 2 A325 BOLTS W/ WASHERS @ CL-18 TO E/W INT. COLUMN CONNECTION (TIGHTEN NUT AGAINST UNTHREADED PORTION OF BOLT. THIS CONNECTION IS DESIGNED TO MOVE.)

SHEETING PACKAGE MK# EW04S-A



SHEETING PACKAGE MK# EW03S-B

| FRAMED OPENING FLASHING SCHEDULE | | | | |
|----------------------------------|-------------|----------|-----------|------|
| QTY. | SIZE | HEADER | JAMBS | SILL |
| 1 | 8'0" X 8'0" | FDH2-8.3 | FJ15B-8.2 | N/A |

| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | SCALE: | NONE | DATE | CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
|-----|-----------|------|------|------|------|------|-----|-----------|------|------|------|------|------|--------------------------|--------|----------|----------|----------------------|---------|----------|
| | | | | | | | | | | | | | | [] FOR CONSTRUCTION | | | | 3129 MAIN | | |
| | | | | | | | | | | | | | | [] FOR APPROVAL | | | | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | [] FOR PERMIT ONLY | | | | HORIZON CONSTRUCTION | | |
| | | | | | | | | | | | | | | [] FOR PRELIMINARY | | | | | | |
| | | | | | | | | | | | | | | CAD BY: | BJC | 08/25/08 | 15:53:02 | Ver. 30.1 | | |

The diagram shows the elevation of a bridge deck with a total height of 22'0". The vertical dimensions on the left are 2'0", 5'6", 4'0", 2'6", 2'6", and 5'6". The horizontal dimensions at the bottom are 1'0", 19'0", 20'0", 17'10", and 2'2". The deck is divided into three sections by vertical lines. The first section is 19'0" wide and contains reinforcement 8Z16-21.0KK (3) REQUIRED TYPICAL U.N. The second section is 20'0" wide and contains reinforcement 8Z16-22.0KK (3) REQUIRED TYPICAL U.N. The third section is 17'10" wide and contains reinforcement 8Z16-21.0KJ (3) REQUIRED TYPICAL U.N. The deck is supported by four piers, numbered 1, 2, 3, and 4. The total width of the bridge is 60'0" OUT TO OUT OF GIRT LINE. The deck is labeled "MASONRY NOT BY ABC". The reinforcement details are as follows:

- D1 - DJ80C6-7.1 1/2AP1
- D2 - DJ80C6-7.1 1/2BP1

* BLDG 2 GIRT LINE • REW

The drawing shows a cross-section of a building with a total height of 22'7 1/2". The structure includes a roof with a 1/2HH pitch, girts (1B95S3, 1B95S2), joists (8Z16, 8Z15, 8Z13), and floor joists (DH80C4). It also shows interior columns (D3, D4), a central stairwell (CL-7), and various wall and ceiling materials (Z1, Z2, Z3). Dimensions for spans and heights are provided throughout the section.

RR-0101

1B95S3-17.1 1/2HH

1B95S2-24.11 1/2HK

6'1 1/2

5'0

4'0

7'6

22'7 1/2

CL-2(TYP)

8Z16-15.11ZK

8Z16-26.0KK

8Z15-15.11ZK

8Z13-26.0KK

DH80C4-10.0

DH80C4-10.0

DH80C4-10.0

CL-7 (TYP)

Z1

D3

D4

100' X 100' F.O.

100' X 100' F.O.

100' X 100' F.O.

Z2

D3

D4

CL-3 (TYP)

3'0

10'0

2'0

2'0

10'0

5'0

10'0

1'0

2'2

15'0

24'0

40'0 OUT TO OUT OF GIRT LINE (BLDG 3)

4

5

6

Z1 - 8Z16-3.6 1/2P1

Z2 - 8Z16-0.6 1/2ZZ

D3 - DJ80C4-11.1 1/2AP1

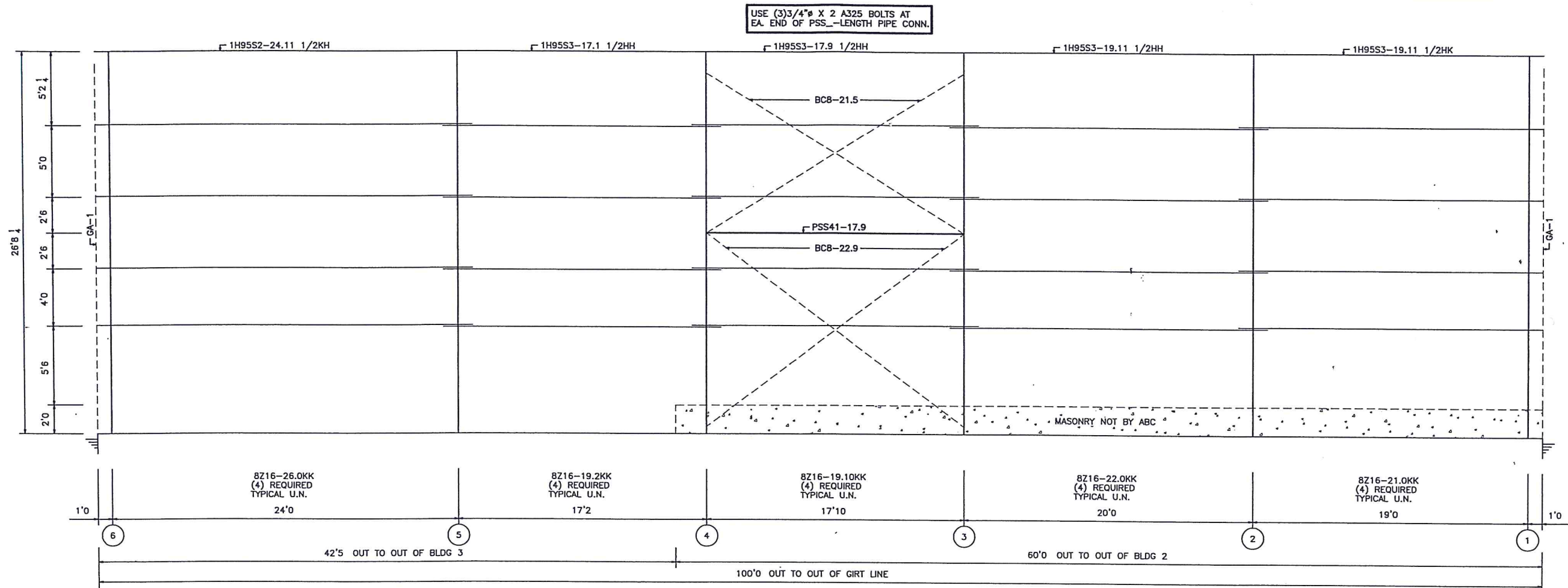
D4 - DJ80C4-11.1 1/2BP1

[illegible]

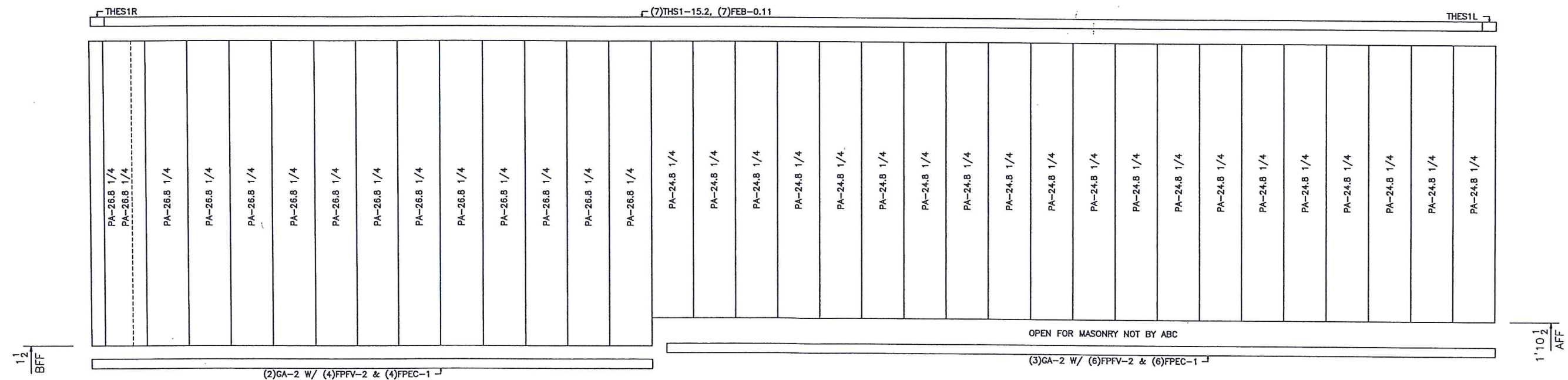
| FRAMED OPENING FLASHING SCHEDULE | | | | |
|----------------------------------|-------------|-----------|------------|------|
| QTY. | SIZE | HEADER | JAMBS | SILL |
| 1 | 3'4 X 7'2 | FDH2-3.7 | FJ15B-7.4 | N/A |
| 3 | 10'0 X 10'0 | FDH2-10.3 | FJ15B-10.2 | N/A |

SHEETING PACKAGE MK# SW01S-B


[illegible]

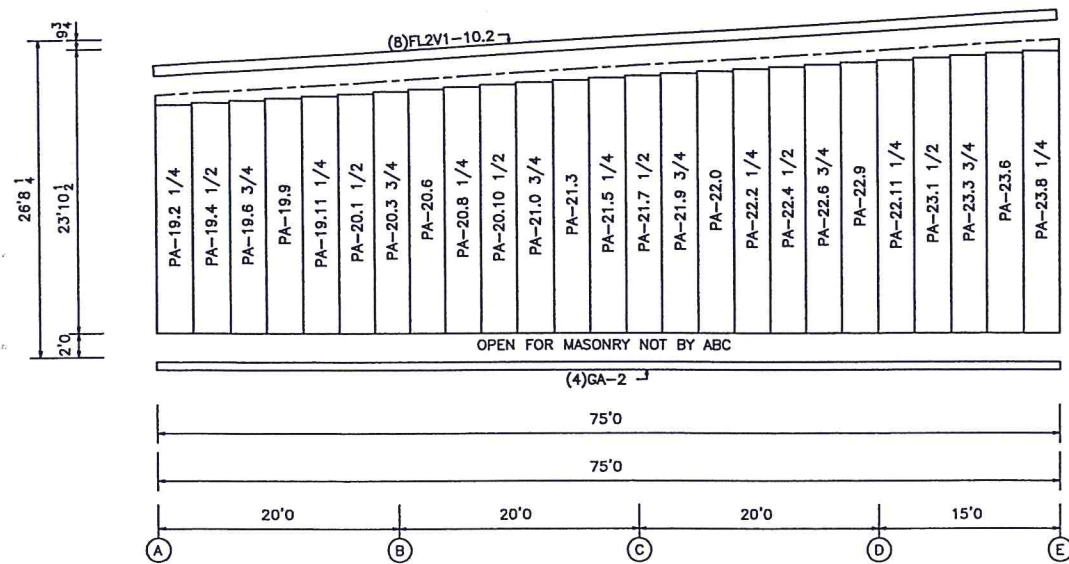


SIDEWALL FRAMING PLAN AT LINE G
GIRT LAPS = 2'-0" TYPICAL U.N.

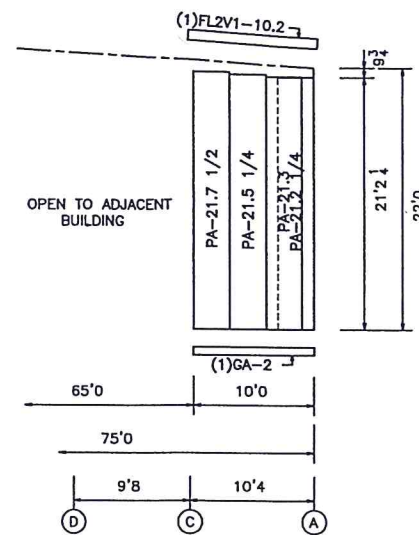


SHEETING PACKAGE MK# SW02S-A

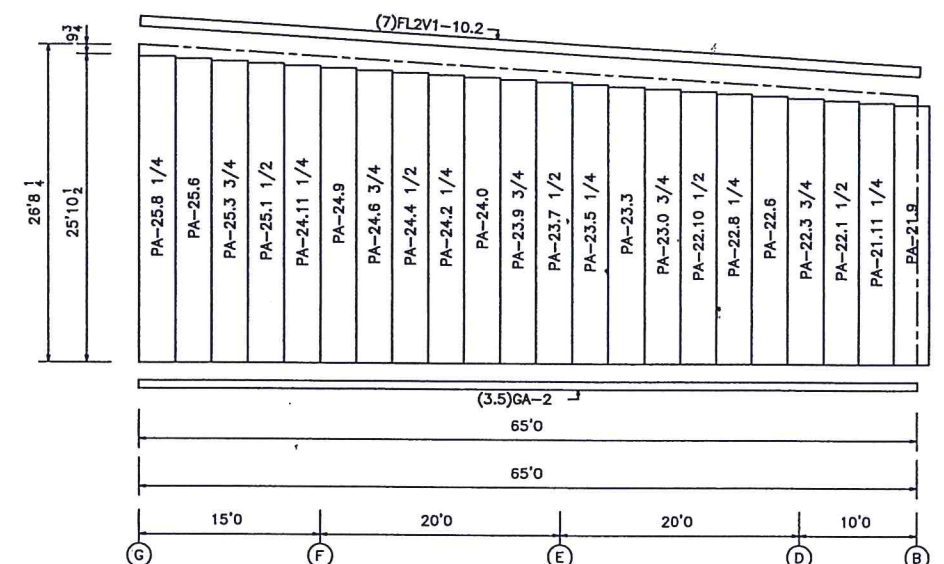
| REVISIONS | | | | | | REVISIONS | | | | | | DRAWING SUBMITTAL STATUS | | | SCALE: NONE | | DATE | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | EL PASO ILLINOIS | |
|-----------|------|------|------|------|------|-----------|------|------|------|------|------|--------------------------|-----------------|------|---|----------------------------|-------------------------------|--|------------------|--|
| NO. | MADE | CK'D | DATE | ENGR | DATE | NO. | MADE | CK'D | DATE | ENGR | DATE | [] FOR CONSTRUCTION | DRAWN BY: | DATE |  | AMERICAN BUILDINGS COMPANY | DRAWING NUMBER: 61874601 E-10 | | REV.NO. 0 | |
| | | | | | | | | | | | | [] FOR APPROVAL | CHECKED BY: | DATE | | | | | | |
| | | | | | | | | | | | | [] FOR PERMIT ONLY | DESIGN APPD BY: | DATE | | | | | | |
| | | | | | | | | | | | | [] FOR PRELIMINARY | CAD BY: | DATE | | | | | | |
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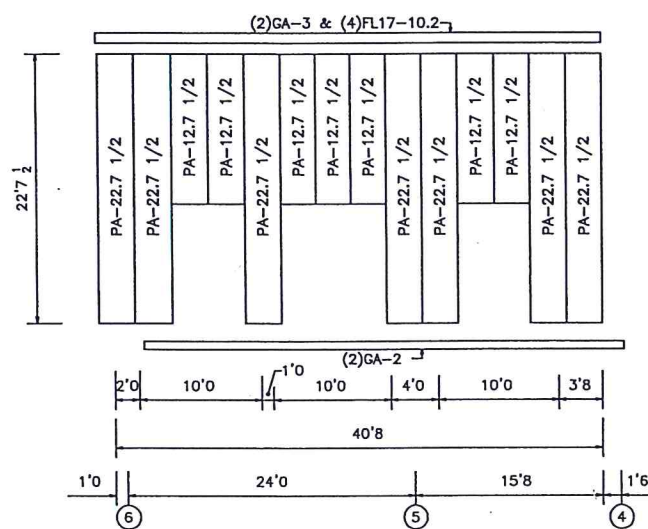
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LINER PANEL • LINE 1



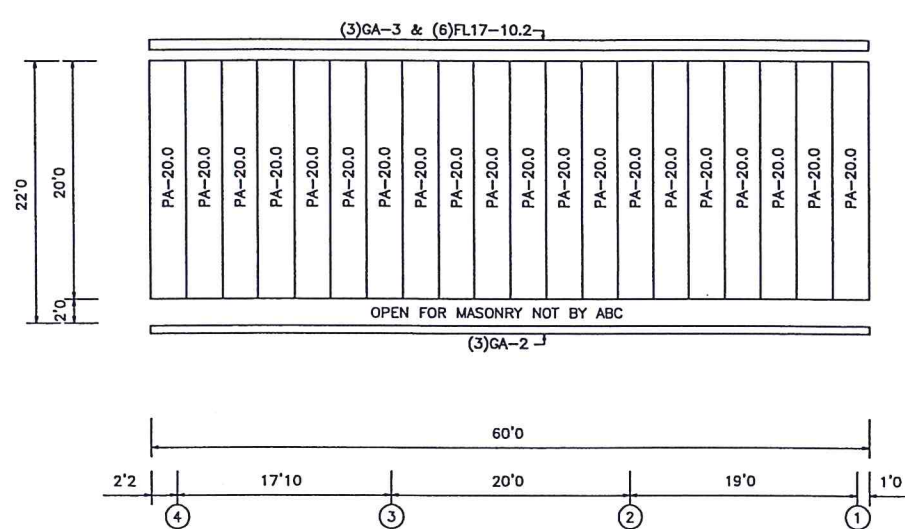
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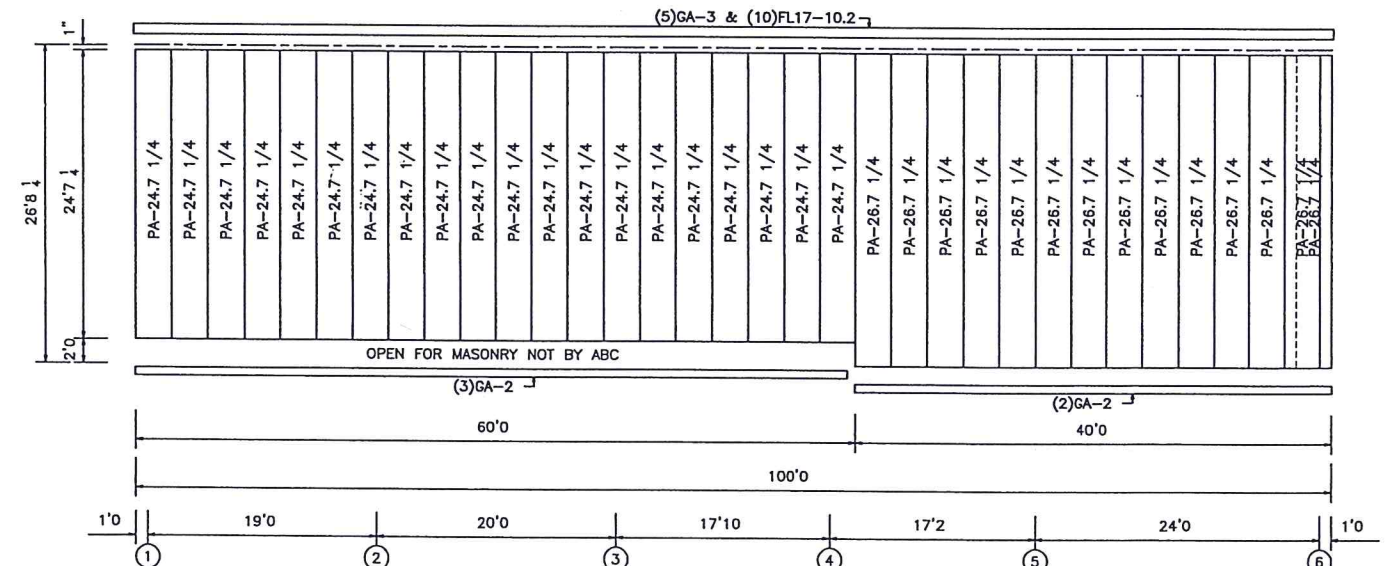
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LINER PANEL • LINE 6



SHEETING PACKAGE MK# SW01L-B
LINER PANEL • LINE B



SHEETING PACKAGE MK# SW01L-A
LINER PANEL • LINE A



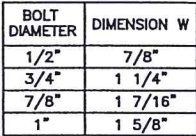
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LINER PANEL • LINE G

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| | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | | | | 3129 MAIN | | |
| | | | | | | | | | | | | | | [] FOR APPROVAL | | | | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | [] FOR PERMIT ONLY | | | | HORIZON CONSTRUCTION | | |
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| BUILDING ALIGNMENT | GE01 |
| | AA |

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| SAFETY COMMITMENT | GE70 |
| | AA |



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| ASTM A325 BOLT IDENTIFICATION | MF01 |
| | AA |

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| BOLT INSTALLATION & INSPECTION NOTES | MF91 |
| 1/2"φ, 3/4"φ, 7/8"φ & 1"φ STRUCTURAL BOLTS (A325) | AA |

[illegible]



GE01
AA

GE70
AA

MF01
AA

MF91
AA[illegible]



SAFETY PRECAUTION
STAY WELL IN CLEAR OF ALL LOADS
BEING MOVED BY ANY TYPE OF
LIFTING DEVICE. KEEP HANDS AND FEET
CLEAR OF MOVING LOADS.

BUILDING ALIGNMENT

GE01
AA

SAFETY NOTES:

AMERICAN BUILDINGS COMPANY STRONGLY RECOMMENDS THAT SAFE WORKING CONDITIONS AND ACCIDENT PREVENTION PRACTICES BE THE TOP PRIORITY ON ANY JOB SITE.

LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS SHOULD ALWAYS BE FOLLOWED TO HELP INSURE WORKER SAFETY.

MAKE CERTAIN ALL EMPLOYEES KNOW THE SAFEST AND MOST PRODUCTIVE WAY OF ERECTING A BUILDING. EMERGENCY TELEPHONE NUMBERS, LOCATIONS OF FIRST AID STATIONS AND EMERGENCY PROCEDURES SHOULD BE KNOWN TO ALL EMPLOYEES.

DAILY MEETINGS HIGHLIGHTING SAFETY PROCEDURES, THE USE OF HARD HATS, RUBBER SOLE SHOES FOR ROOF WORK, PROPER EQUIPMENT FOR HANDLING MATERIAL AND SAFETY NETS WHERE POSSIBLE ARE RECOMMENDED ERECTION PRACTICES.

AMERICAN BUILDINGS COMPANY INTENDS THAT THESE DRAWINGS BE INTERPRETED AND ADMINISTERED WITH SOUND JUDGEMENT CONSISTENT WITH GOOD SAFETY PRACTICES.

ALL SAFETY PRECAUTIONS, OSHA SAFETY REQUIREMENTS, OR ANY OTHER APPROPRIATE SAFETY REQUIREMENTS, CUSTOMARY OR STATUTORY, MUST BE ADHERED TO, TO INSURE MAXIMUM WORKER SAFETY.

IF OIL OR OTHER SLIPPERY SUBSTANCES ARE SPILLED ON THE ROOF PANELS, WIPE THEM OFF IMMEDIATELY TO PREVENT SLIPPING OR FALLING.

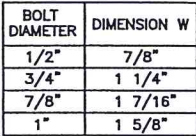
YOU SHOULD MAINTAIN A FIRM, SAFE POSITION WHEN USING ANY TOOL.

YOU SHOULD MAINTAIN A CONSTANT AWARENESS OF YOUR LOCATION IN RELATION TO THE ROOF EDGE WHEN USING TOOLS AND MACHINES OR PERFORMING ANY OTHER FUNCTION ON THE ROOF AREA.

DO NOT UNDER ANY CIRCUMSTANCES STEP OR WALK ON THE SURFACE OF ANY FIBERGLASS SKYLIGHT. IF FOOT TRAFFIC IS NECESSARY OVER SKYLIGHT, USE WALK BOARDS THAT ARE PROPERLY SUPPORTED BY THE BUILDING PURLINS.

SAFETY COMMITMENT

GE70
AA



ASTM A325 BOLT IDENTIFICATION

MFO1
AA

BOLTED JOINTS:

BOLTED JOINTS SHALL BE CONNECTED AND INSPECTED IN ACCORDANCE WITH THE "SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS", JUNE 23, 2000, RESEARCH COUNCIL ON STRUCTURAL CONNECTIONS. UNLESS NOTED OTHERWISE ON THE ABC ERECTION DRAWINGS, ALL A325 BOLTS ARE USED IN CONNECTIONS DEFINED AS SNUG-TIGHT JOINTS (ST). FOR INSTALLATION IN SNUG-TIGHT JOINTS, ALL BOLT HOLES SHALL BE ALIGNED TO PERMIT INSERTION OF THE BOLTS WITHOUT UNDUE DAMAGE TO THE THREADS. BOLTS SHALL BE IN ALL HOLES WITH THREADS TRIMMED BEFORE COMING INTO CONTACT WITH THE SNUG-TIGHT JOINT. THE PROGRESSIVE SYSTEMATICALLY WORKING FROM THE JOINT TO THE END OF THE SNUG TIGHT CONDITION IS THE TIGHTNESS THAT IS ATTAINED WITH A FEW IMPACTS OF AN IMPACT WRENCH OR THE FULL EFFORT OF AN IRONWRENCH USING AN ORDINARY SPUD WRENCH SO THAT THE CONNECTED PLIES ARE SOLIDLY SEATED AGAINST EACH OTHER, BUT NOT NECESSARILY IN CONTINUOUS CONTACT. MORE THAN ONE CYCLE THROUGH THE BOLT PATTERN MAY BE REQUIRED TO ACHIEVE THE SNUG-TIGHT CONDITION.

CONNECTIONS WITH A320 BOLTS WHICH ARE DEFINED AS PRETENSIONED (PT) OR SLIP-CRITICAL (SC) JOINTS WILL BE NOTED AS SUCH ON THE ABC DRAWINGS OR BY THE ENGINEER OF RECORD. ALL CONNECTIONS WITH A490 BOLTS ARE EITHER PRETENSIONED (PT) OR SLIP-CRITICAL (SC) JOINTS. PRETENSIONED JOINTS ARE TYPICALLY REQUIRED WHEN THE JOINT IS SUBJECT TO SIGNIFICANT LOAD REVERSAL, THE JOINT IS SUBJECT TO FATIGUE LOAD WITH NO LOAD REVERSAL, THE BOLTS ARE SUBJECT TO TENSILE FATIGUE, THE BUILDING SUPPORTS A CRANE OF OVER 5-TON CAPACITY, OR THE CONNECTION IS PART OF THE SEISMIC LOAD RESISTING SYSTEM AND THE BUILDING IS CLASSIFIED AS SEISMIC DESIGN CATEGORY E, OR F. NOTE THAT LOADS FROM WIND ARE NOT CONSIDERED SIGNIFICANT LOAD REVERSAL. WHERE SIGNIFICANT LOADS ARE LOADING SLIP CRITICAL JOINTS ARE REQUIRED WHEN SLIP IS DETERMINED TO BE DETRIMENTAL TO THE PERFORMANCE OF THE STRUCTURE. INSTALLATION METHODS PERMITTED FOR PRETENSIONED AND SLIP-CRITICAL JOINTS INCLUDE TURN OF THE NUT PRETENSIONING, CALIBRATED WRENCH PRETENSIONING, TWIST-OFF-TYPE TENSION CONTROL BOLT ASSEMBLIES, AND DIRECT-TENSION-INDICATOR PRETENSIONING. PRETENSION VALUES EQUAL TO OR GREATER THAN THE VALUES LISTED IN THE BOLT PRETENSION SCHEDULE SHALL BE PROVIDED.

FOR INSTALLATION BY TUN-OF-NUT PRETENSIONING, ALL BOLTS SHALL FIRST BE TIGHTENED IN ACCORDANCE WITH THE SNUG-TIGHT PROCEDURE. THE NUT OR HEAD AS APPLICABLE SHALL THEN BE ROTATED BY THE AMOUNT SPECIFIED IN THE BOLT PRETENSION SCHEDULE PROGRESSING SYSTEMATICALLY FROM THE MOST RIGID PART OF THE JOINT. THE PART NOT TURNED BY THE WRENCH SHALL BE PREVENTED FROM ROTATING DURING THIS OPERATION.

| ASTM A325 BOLT PRETENSION SCHEDULE | | | |
|------------------------------------|--------------------------------------|--|------------------------|
| BOLT DIA d_b | SPECIFIED MINIMUM BOLT PRETENSION | NUT OR HEAD ROTATION FROM SNUG-TO TIGHT CONDITION | |
| | | $L_b \leq 4d_b$ | $4d_b < L_b \leq 8d_b$ |
| 1/2" ϕ | 12 KIPS | 1/3 TURN | 1/2 TURN |
| 3/4" ϕ | 28 KIPS | | |
| 7/8" ϕ | 39 KIPS | | |
| 1" ϕ | 51 KIPS | | |

INSPECTION:


INSPECTION REQUIREMENTS FOR SNUG-TIGHT JOINTS CONSISTS OF VERIFICATION THAT THE PROPER FASTENER COMPONENTS WERE USED AND THAT THE CONNECTED ELEMENTS WERE FABRICATED PROPERLY. AFTER ASSEMBLY, IT SHALL BE VISUALLY ENSURED THAT THE PILES ARE SOLIDLY SEATED AGAINST EACH OTHER, BUT NOT NECESSARILY IN CONTINUOUS CONTACT, AND THAT WASHERS, IF REQUIRED, HAVE BEEN USED. NO FURTHER EVIDENCE OF CONFORMITY IS REQUIRED.

FOR TURN-OF-NUT PRETENSIONING, IN ADDITION TO THE INSPECTION REQUIREMENTS FOR SNUG-TIGHT JOINTS, THE INSPECTOR SHALL OBSERVE THE PRE-INSTALLATION VERIFICATION TESTING AND MONITOR THE WORK IN PROGRESS TO ENSURE THAT THE BOLTING CREW PROPERLY ROTATES THE TURNED ELEMENT BY THE AMOUNT SPECIFIED IN THE SCHEDULE. ALTERNATIVELY, WHEN THE FASTENERS ARE MARKED AFTER THE SNUG-TIGHT (SNUG-TIGHT CONDITION) VISUAL INSPECTION IS PERMITTED, THE SIDE OF NUTS AND BOLTS THAT HAVE BEEN IMPACTED SUFFICIENTLY TO INDUCE THE MINIMUM PRETENSION LOADS WILL APPEAR SLIGHTLY PEENED, NO FURTHER EVIDENCE OF CONFORMITY IS REQUIRED.

BOLT INSTALLATION & INSPECTION NOTES

1/2"Ø, 3/4"Ø, 7/8"Ø & 1"Ø STRUCTURAL BOLTS (A325)

MF91
AA

| REVISIONS | | | | | | | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | | | | | | | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | | SCALE: | NONE | DATE | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | EL PASO ILLINOIS | | |
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| | | | | | | | | | | | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | DRAWN BY: | BJC | | AMERICAN BUILDINGS COMPANY  | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | [] FOR APPROVAL | CHECKED BY: | | | | | | | | | |
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SAFETY PRECAUTION
STAY WELL IN CLEAR OF ALL LOADS
BEING MOVED BY ANY TYPE OF
LIFTING DEVICE. KEEP HANDS AND FEET
CLEAR OF MOVING LOADS.

BUILDING ALIGNMENT

GE01
AA

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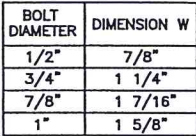
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SAFETY COMMITMENT

GE70
AA



ASTM A325 BOLT IDENTIFICATION

MF01
AA

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|------------------------------------|--------------------------------------|--|------------------------|
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| | | $L_b \leq 4d_b$ | $4d_b < L_b \leq 8d_b$ |
| 1/2" ϕ | 12 KIPS | 1/3 TURN | 1/2 TURN |
| 3/4" ϕ | 28 KIPS | | |
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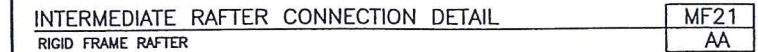
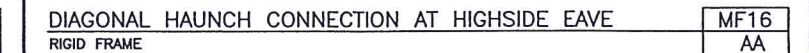
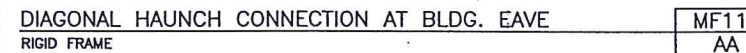
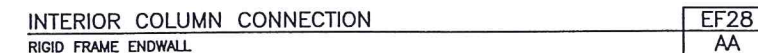
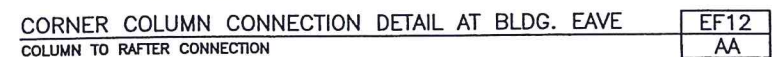
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BOLT INSTALLATION & INSPECTION NOTES

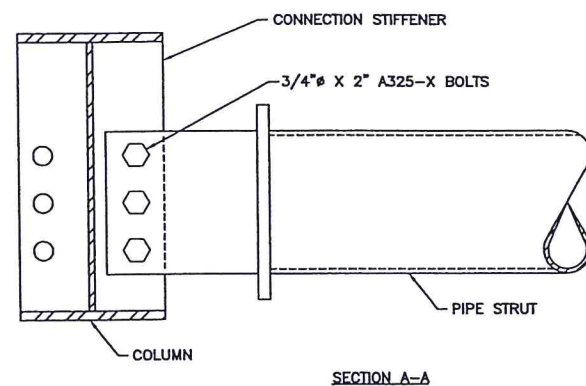
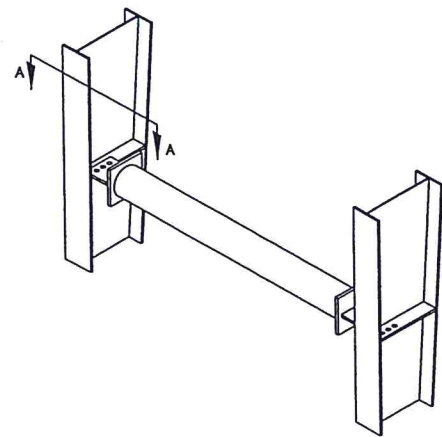
1/2"Ø, 3/4"Ø, 7/8"Ø & 1"Ø STRUCTURAL BOLTS (A325)

MF91
AA

| REVISIONS | | | | | | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | | | | | | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | | | SCALE: | NONE | | DATE | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION |  AMERICAN BUILDINGS COMPANY | EL PASO | ILLINOIS | | | | | | | | | | | | | | | | | | | | | | | | |
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| | | | | | | | | | | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | | | DRAWN BY: BJC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | [] FOR APPROVAL | | | CHECKED BY: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | [] FOR PERMIT ONLY | | | DESIGN APPD BY: | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| REVISIONS | | | | | | | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | | | | | | | MADE | CK'D | DATE | ENGR | DATE | |
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STIFFENER MOUNTED PIPE STRUT AT COLUMN

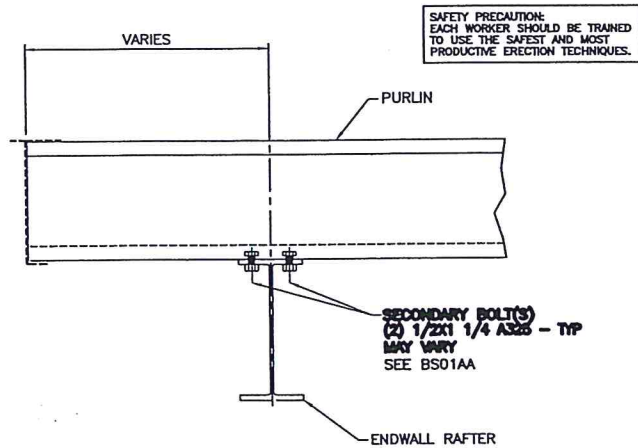
BR15A
AA

| REVISIONS | | | | | | | REVISIONS | | | | | | | DRAWING SUBMITTAL STATUS | | SCALE: | NONE | DATE | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | <div>EL PASO</div> <div></div> <div>ILLINOIS</div> | DRAWING NUMBER | 61874601 SED-003 | REV.NO. 0 |
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| NO. | MADE | CK'D | DATE | ENGR | DATE | NO. | MADE | CK'D | DATE | ENGR | DATE | | | | | | | | | | | | |
| | | | | | | | | | | | | | [*] FOR CONSTRUCTION | DRAWN BY: | BJC | | | | | | | | |
| | | | | | | | | | | | | | [] FOR APPROVAL | CHECKED BY: | | | | | | | | | |
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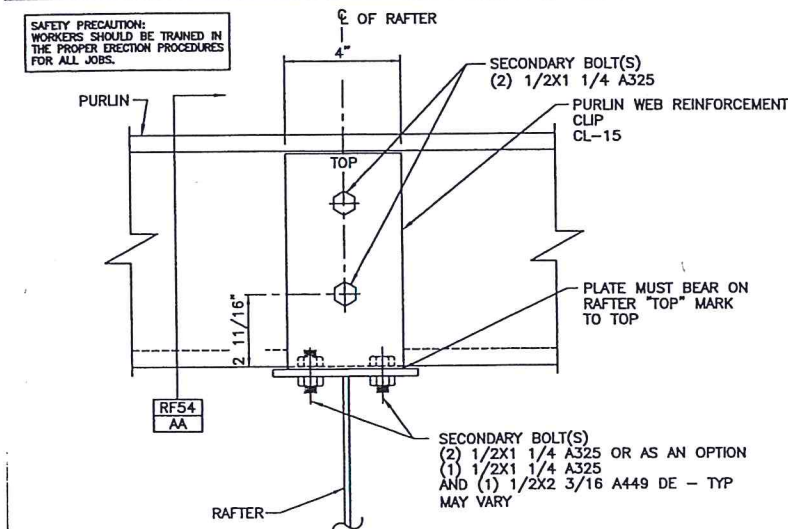
| RAFTER FLANGE THICKNESS | SECONDARY FRAMING GAUGE | DOUBLE ENDED BOLT REQUIRED | 1/2" A325 BOLT REQUIRED FOR LAPPED MEMBERS | 1/2" A325 BOLT REQUIRED FOR NON LAPPED MEMBERS |
|-------------------------|-------------------------|----------------------------|--|--|
| < 3/8" | 11 - 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 15 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 14 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 13 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 12 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
| | 11 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
| | 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
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| | 12 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
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NOTE:
DUE TO TOTAL THICKNESS, LAPPED PURLINS REQUIRE THE USE OF WELDED RAFTER CLIPS.

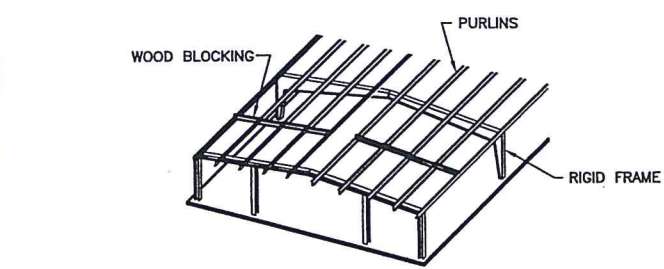
BOLT REQUIREMENTS FOR SECONDARY FRAMING CONNECTIONS AT RAFTERS BS01 AA



PURLIN CONNECTION DETAIL AT ENDWALL FRAME BYPASS, INSET & FLUSH CONDITION RF03R AA



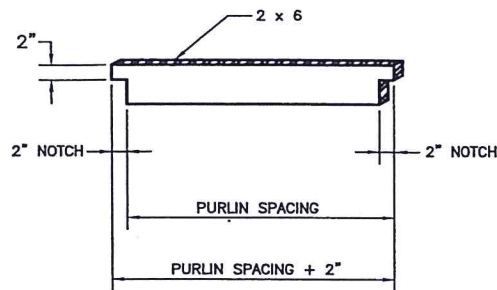
PURLIN WEB REINFORCEMENT CLIP DETAIL RF53 AA



STRAIGHT PURLINS ARE A NECESSITY. ZEE SECTIONS HAVE A NATURAL TENDENCY TO ROLL OUT OF PLANE AND DEFLECT HORIZONTALLY. THIS MUST BE CORRECTED BY FORCING THE PURLINS INTO PROPER PLANE AND SPACING. WOOD BLOCKING IS RECOMMENDED AS ONE METHOD TO ACCOMPLISH THIS.

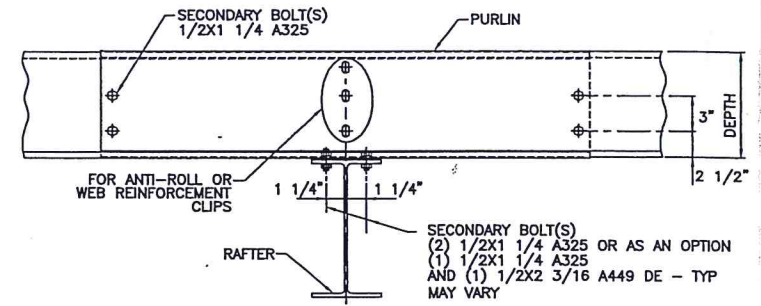
BEFORE SHEETING BEGINS IN ANY BAY, PLACE BLOCKING IN THAT BAY ACROSS THE FULL WIDTH OF THE BUILDING. AT LEAST ONE ROW IN THE CENTER OF THE BAY SHOULD ALWAYS BE USED. USE ADDITIONAL ROWS OF BLOCKING IF NEEDED TO MAINTAIN STRAIGHT PURLINS. ALLOWING THE PURLIN TO ROTATE OR SWEEP OUT OF PLANE COULD PREVENT THE ROOF PANEL FROM PROPERLY ATTACHING TO THE PURLINS AND HAS THE EFFECT OF LOWERING THE LOAD BEARING CAPACITY OF THESE ROLLED PURLINS LEADING TO POTENTIAL PURLIN FAILURE UNDER MAXIMUM DESIGN LOADS.

SAFETY PRECAUTION
DON'T ALLOW BLOCKING TO BE A FALLING HAZARD TO THOSE BENEATH THE ROOF. WORKERS SHOULD WEAR OSHA APPROVED HARD HATS.



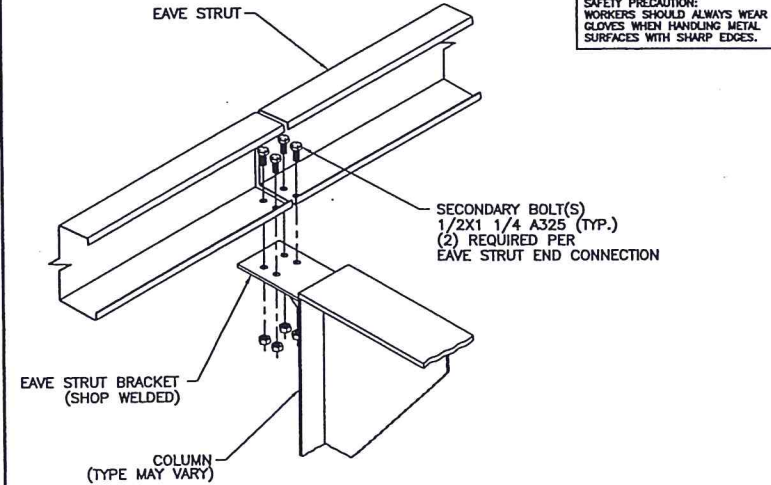
TYPICAL CONSTRUCTION OF THE WOOD BLOCKING IS SHOWN ABOVE. A 2 X 6 MINIMUM BOARD SIZE SHOULD BE USED. REFER TO THE CROSS SECTION FRAMING DRAWINGS THAT ACCOMPANIED THE BUILDING TO DETERMINE THE PURLIN SIZE AND SPACINGS.

PURLIN BLOCKING GE02 AA

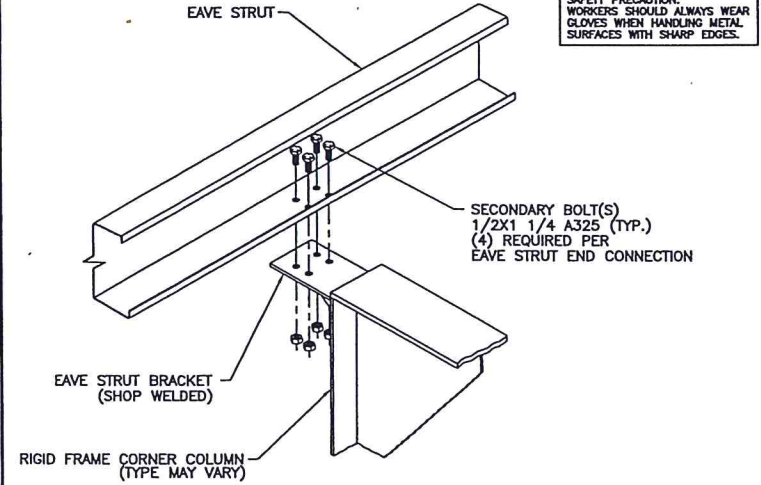


SAFETY PRECAUTION:
PUT WORKER SAFETY FIRST.

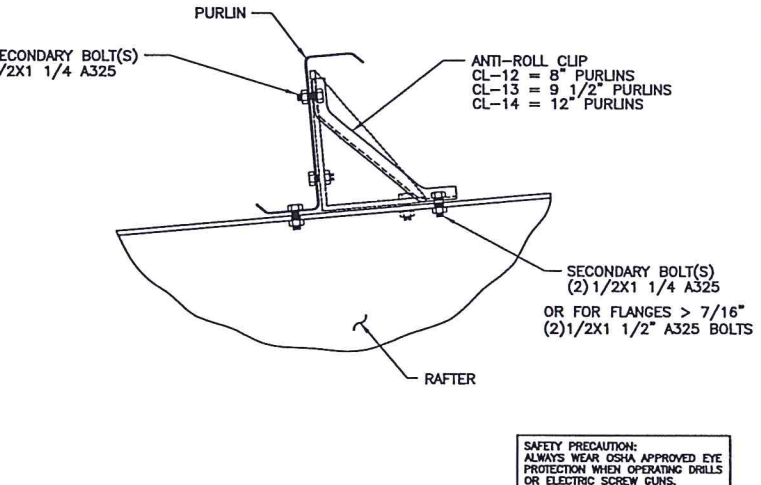
STANDARD PURLIN LAP BYPASS CONDITION RF01 AA



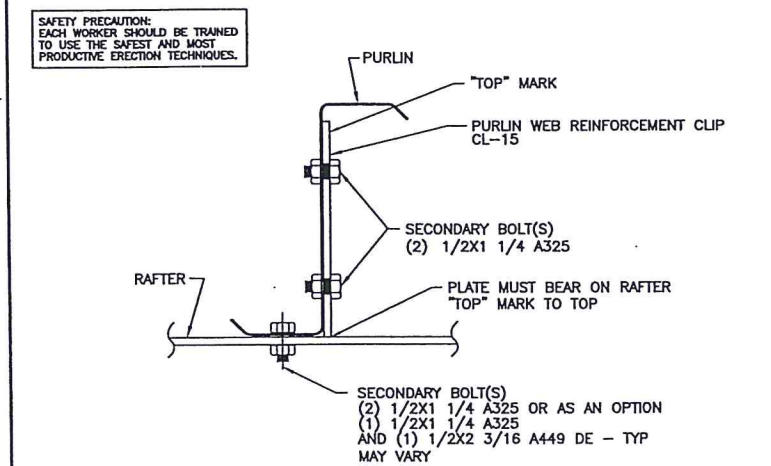
EAVE STRUT CONNECTION (BYPASS GIRTS) LOW SIDE SHOWN / HIGH SIDE SIMILAR RF31R AA



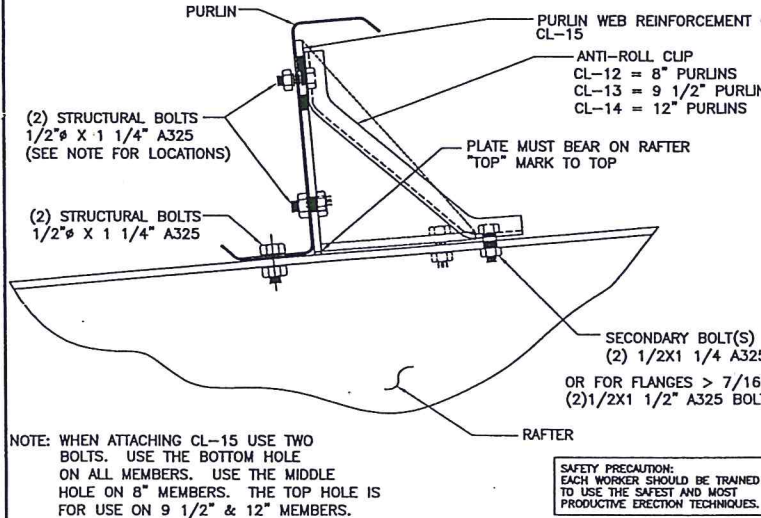
EAVE STRUT CONNECTION (BYPASS GIRTS) LOW SIDE SHOWN / HIGH SIDE SIMILAR RF32R AA



ANTI-ROLL DETAIL UPHILL RIGID FRAME RAFTER RF50 AA

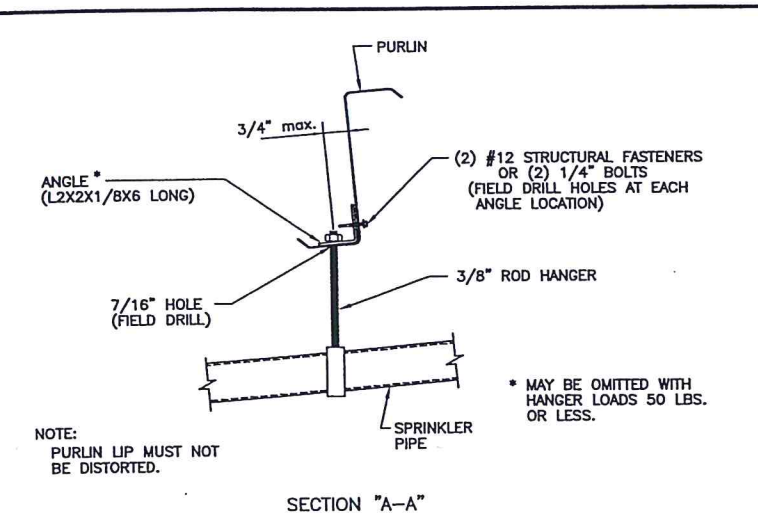


PURLIN WEB REINFORCEMENT CLIP DETAIL RF54 AA



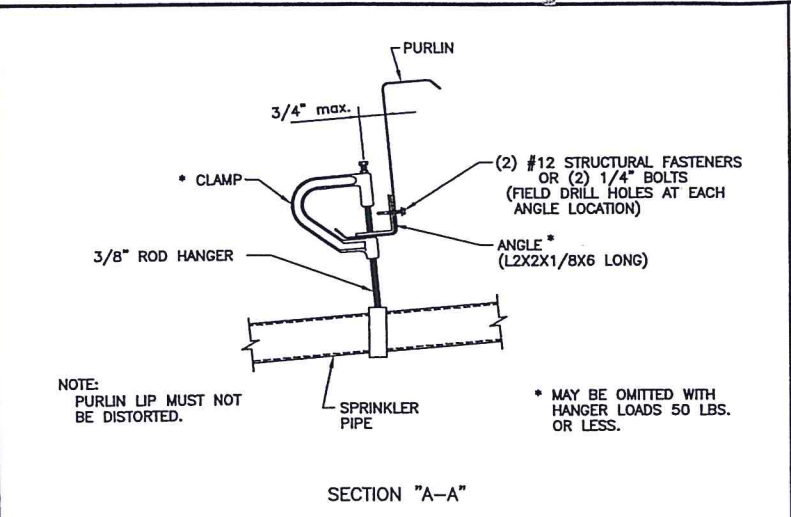
ANTI-ROLL CLIP DETAIL PURLIN WEB REINFORCEMENT CLIP REQUIRED RF57 AA

| NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | 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DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | DATE | | | | | | | ENGR | | | | | | | DATE | | | | | | | NO. | | | | | | | REVISIONS | | | | | | | MADE | | | | | | | CK'D | | | | | | | 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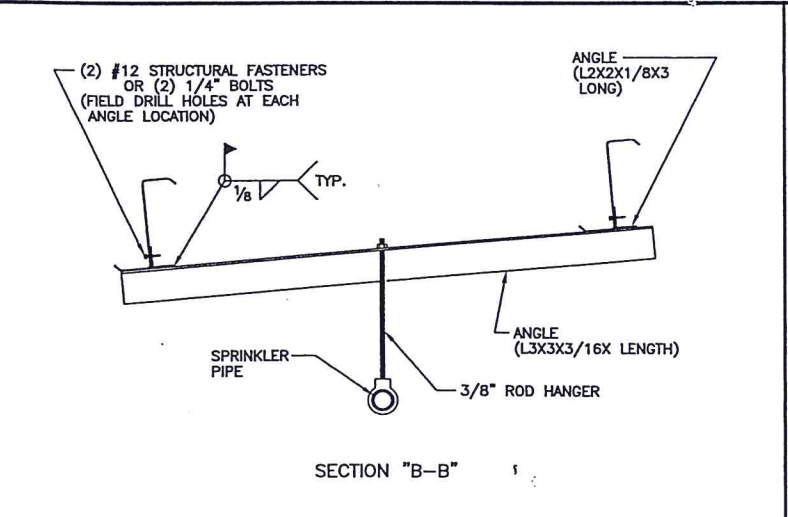
SPRINKLER PIPE HANGER DETAIL AT PURLINS

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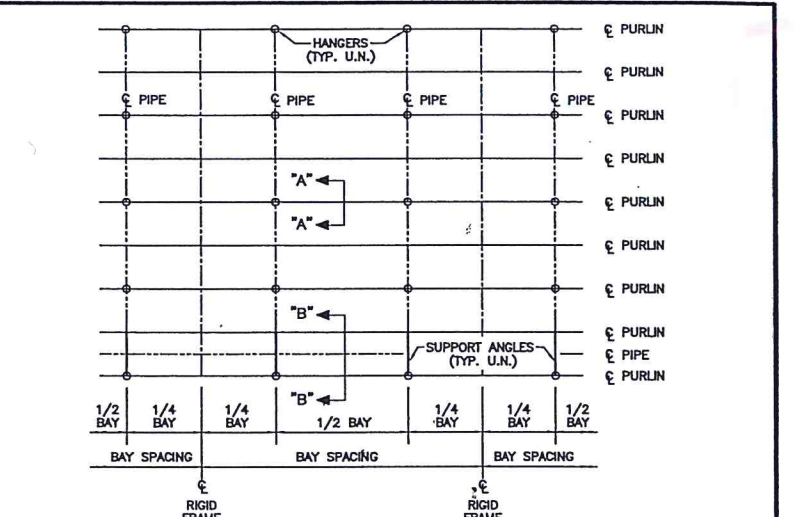
ALTERNATE SPRINKLER PIPE HANGER DETAIL AT PURLINS

SH02
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SPRINKLER PIPE HANGER DETAIL BETWEEN PURLINS

SH03
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TYPICAL SPRINKLER PIPE LAYOUT

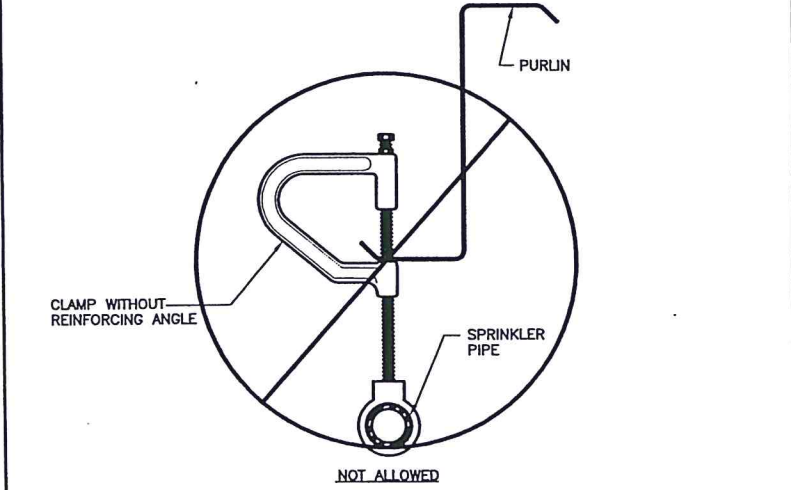
SH04
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| EQUIVALENT COLLATERAL LOAD FOR PIPE SUPPORTS | | | | | | | | | | | |
|--|---------------|---------------|----------|------------|----------|------------|----------|------------|----------------------|------------|------------|
| | | | 20' BAYS | | 25' BAYS | | 30' BAYS | | POINT LOAD AT HANGER | | |
| STD. WT. PIPE SIZE | DRY WT. #/FT. | WET WT. #/FT. | PIPE | PIPE +250# | PIPE | PIPE +250# | PIPE | PIPE +250# | PIPE | PIPE +250# | PIPE +250# |
| 6" | 18.97 | 31.5 | 8 | 11 | 6 | 8 | 5 | 6 | 315 | 565 | |
| 5" | 14.62 | 23.3 | 6 | 9 | 4 | 7 | 3 | 5 | 233 | 483 | |
| 4" | 10.79 | 16.3 | 4 | 7 | 3 | 6 | 2 | 5 | 163 | 413 | |
| 3 1/2" | 9.11 | 13.4 | 4 | 7 | 2 | 5 | 2 | 4 | 134 | 384 | |
| 3" | 7.58 | 10.8 | 3 | 6 | 2 | 5 | 2 | 4 | 108 | 358 | |
| 2 1/2" | 5.79 | 7.9 | 2 | 6 | 1 | 5 | 1 | 4 | 79 | 329 | |
| 2" | 3.65 | 5.1 | 1 | 5 | 1 | 4 | 1 | 3 | 51 | 301 | |
| 1 1/2" | 2.72 | 3.6 | 1 | 5 | 1 | 4 | 1 | 3 | 36 | 286 | |
| 1 1/4" | 2.27 | 2.9 | 1 | 5 | 1 | 4 | 1 | 3 | 29 | 279 | |
| 1" | 1.68 | 2.1 | 1 | 5 | 1 | 4 | 1 | 3 | 21 | 271 | |

NOTES:
1. PIPE HANGERS ASSUMED AT 10' SPACINGS LOCATED PER TYPICAL PIPE LAYOUT AS SHOWN IN DETAIL SH04/AA.
2. FOR PIPE SUPPORTED AT 5' INTERVALS, VALUES ABOVE MAY BE HALVED.
3. PURLIN SPACE ASSUMED TO BE 5' ON CENTERS.
4. SPECIAL CONSIDERATION SHOULD BE MADE WHEN ALL BAYS ARE NOT UNIFORMLY LOADED.
5. 250# LOAD IS REQUIRED BY UBC TO BE APPLIED TO ANY ONE SINGLE SUPPORT IN ADDITION TO PIPE LOADS.

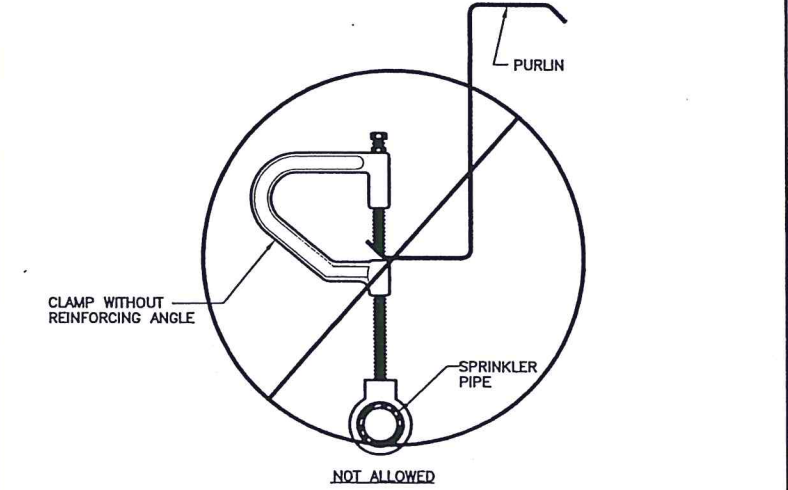
LOADS FOR SPRINKLER PIPE SUPPORTS

SH05
AA



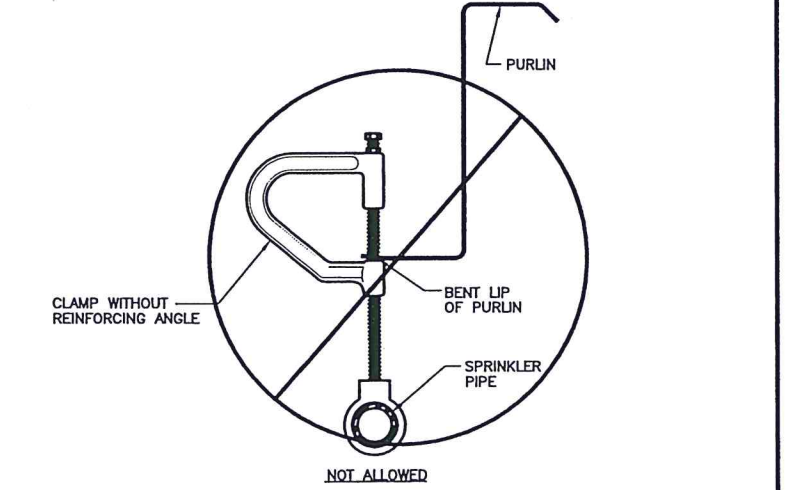
SPRINKLER PIPE "NOT ALLOWED DETAIL"
CLAMP WITHOUT REINFORCING ANGLE

SH06
AA



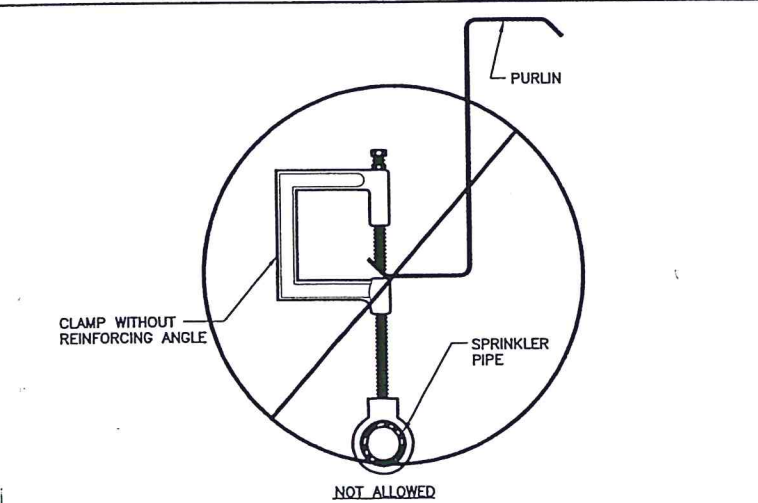
SPRINKLER PIPE "NOT ALLOWED DETAIL"
CLAMP WITHOUT REINFORCING ANGLE AND ATTACHED AT PURLIN LIP

SH06A
AA



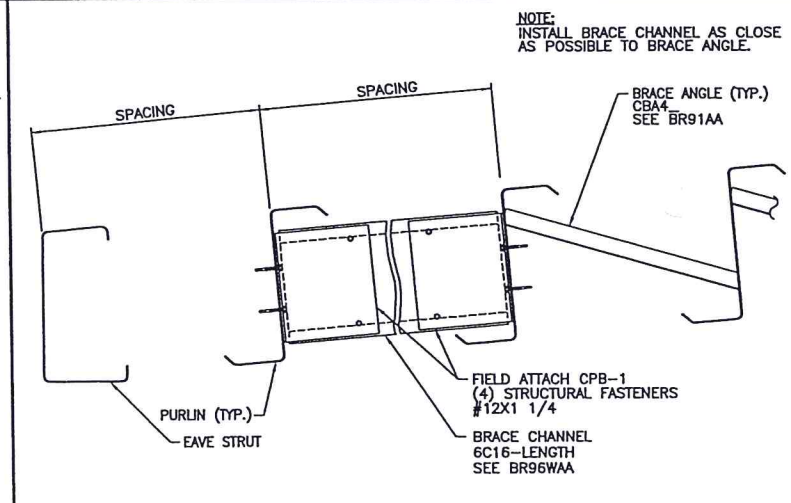
SPRINKLER PIPE "NOT ALLOWED DETAIL"
CLAMP WITHOUT REINFORCING ANGLE AND BENDING LIP OF PURLIN

SH07
AA



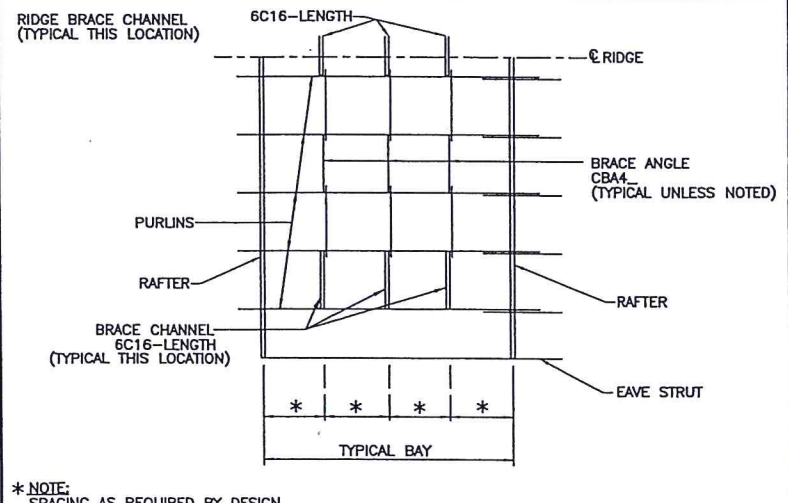
SPRINKLER PIPE "NOT ALLOWED DETAIL"
CLAMP WITHOUT REINFORCING ANGLE AND ATTACHED AT PURLIN LIP

SH07A
AA



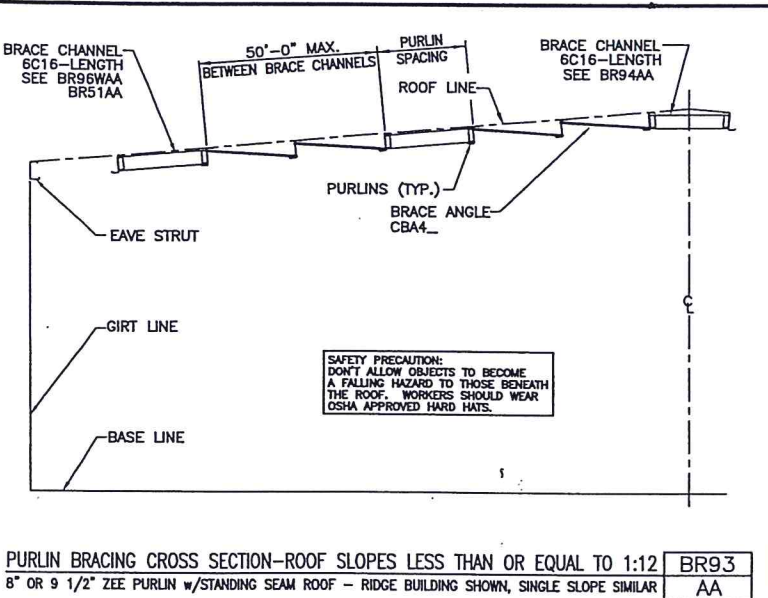
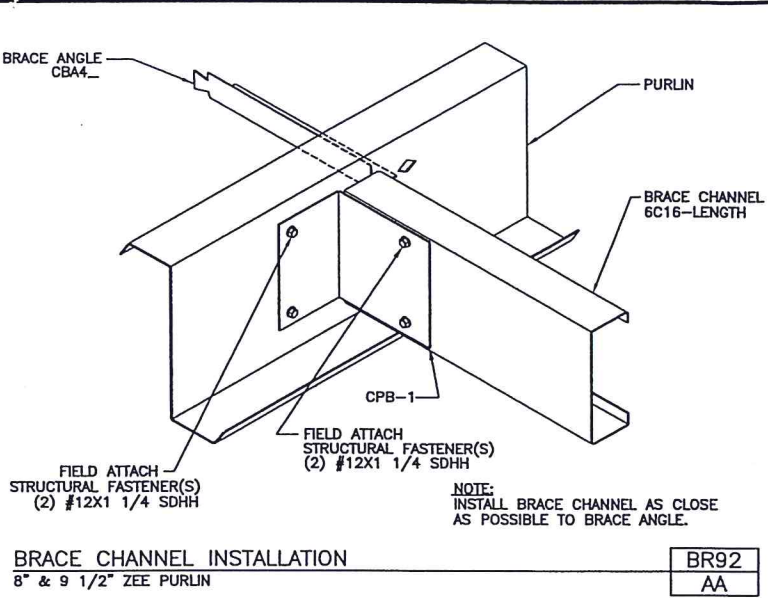
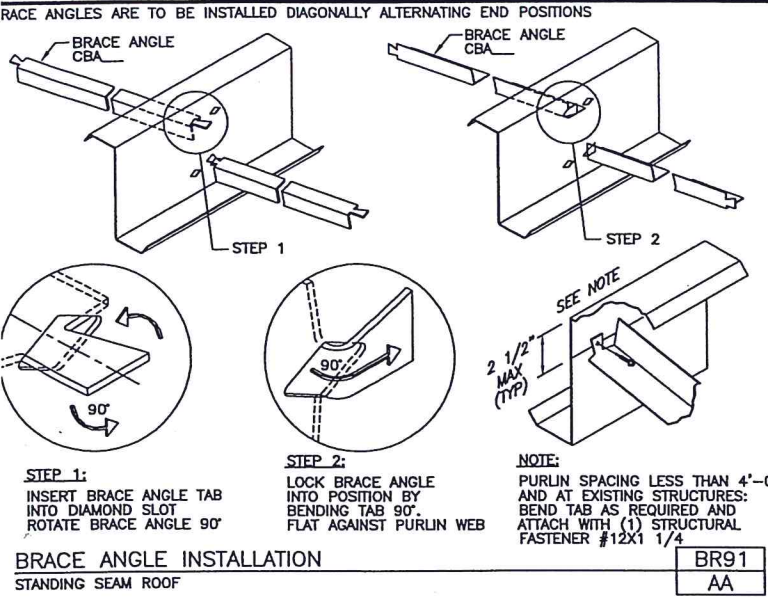
BRACING DETAIL-ROOF SLOPE LESS THAN OR EQUAL TO 1:12
8" & 9 1/2" ZEE PURLINS w/STANDING SEAM ROOF - HIGH SIDE SIMILAR

BR51
AA



PURLIN BRACING KEY PLAN
8" & 9 1/2" ZEE PURLINS w/STANDING SEAM ROOF

BR90
AA

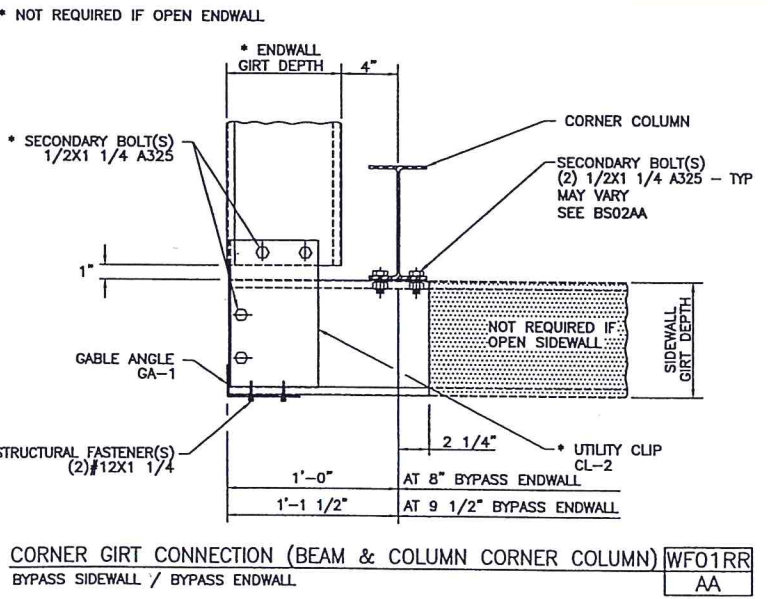
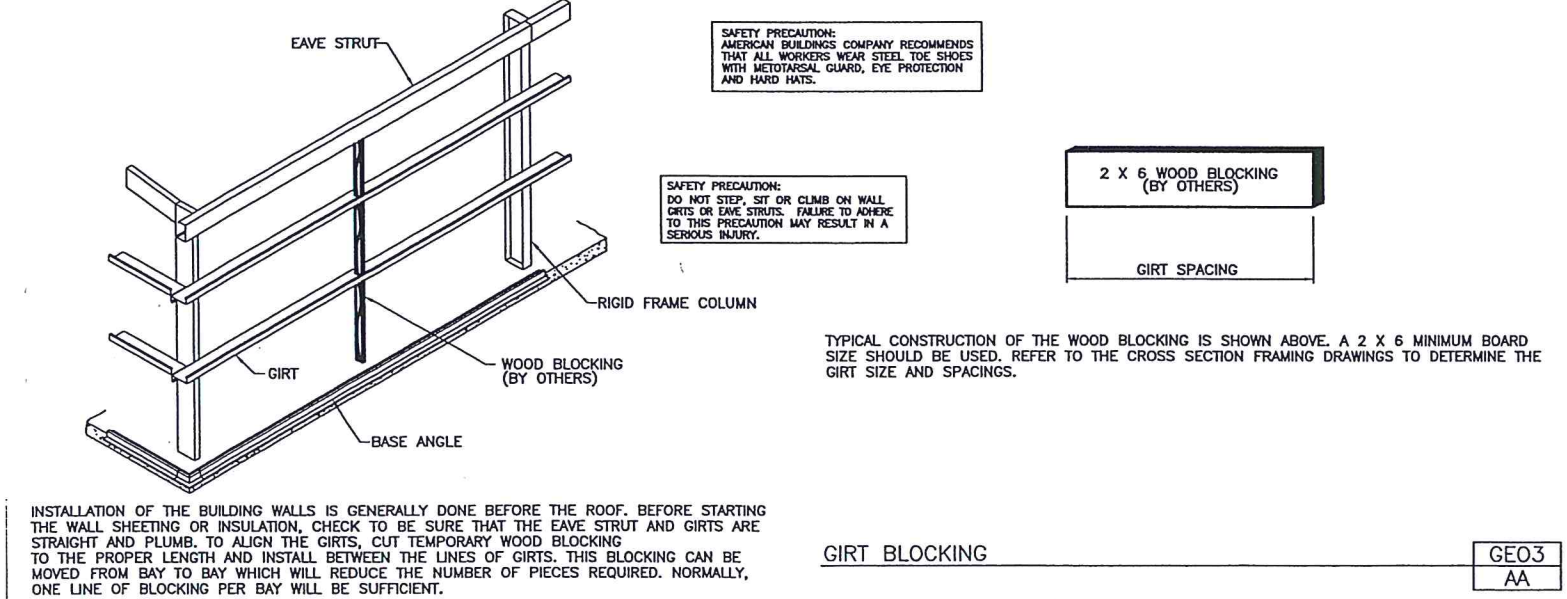
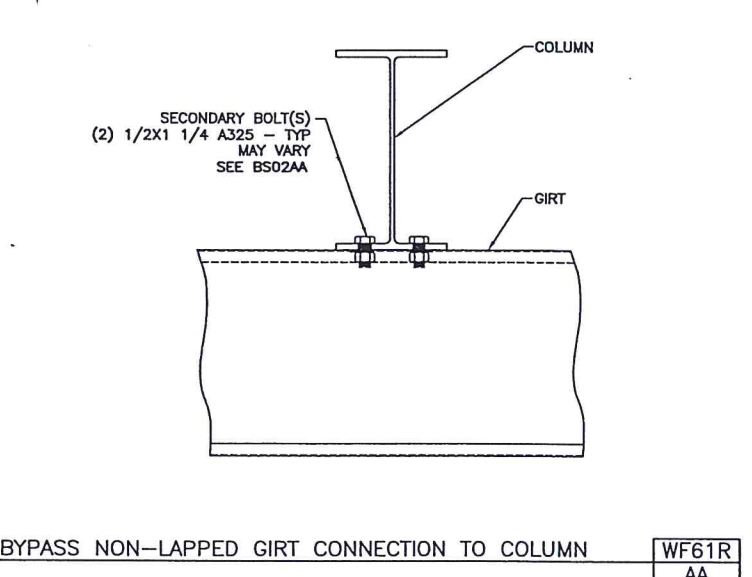
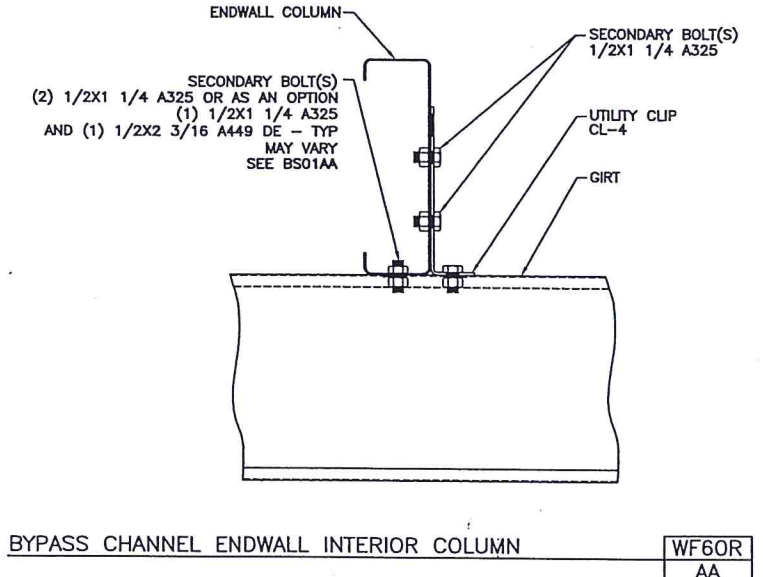
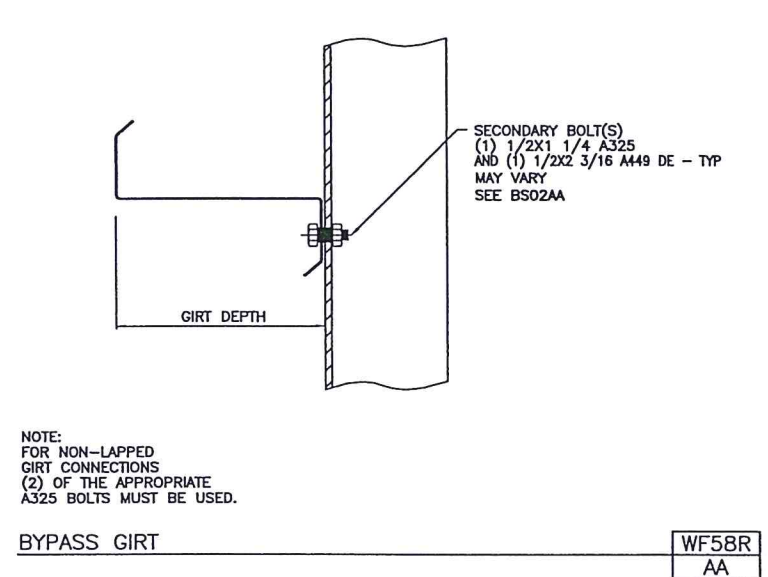



- PURLIN BRACING NOTES
- BR94
AA
1. APPLICABLE TO ROOFS WITH STANDING SEAM II, STANDING SEAM 360, LOC-SEAM, LOC-SEAM 360 AND SEAM-LOC PANELS.
 2. PURLINS ARE TO BE INSTALLED WITH PURLIN WEBS 90° TO RAFTERS.
 3. PURLIN BRACING IS NOT TO DISTORT OR ALTER PURLINS FROM THEIR INTENDED SHAPE AND LOCATION.
 4. SPACING AS REQUIRED BY DESIGN.
 5. FOR FACTORY MUTUAL REQUIREMENTS, PURLIN BRACING IS NOT TO EXCEED 80" O.C. AND IS REQUIRED ONLY IN THE END BAYS OF THE BUILDING.
- (BRACING NOT APPLICABLE WITH SCREW DOWN ROOF SYSTEMS)

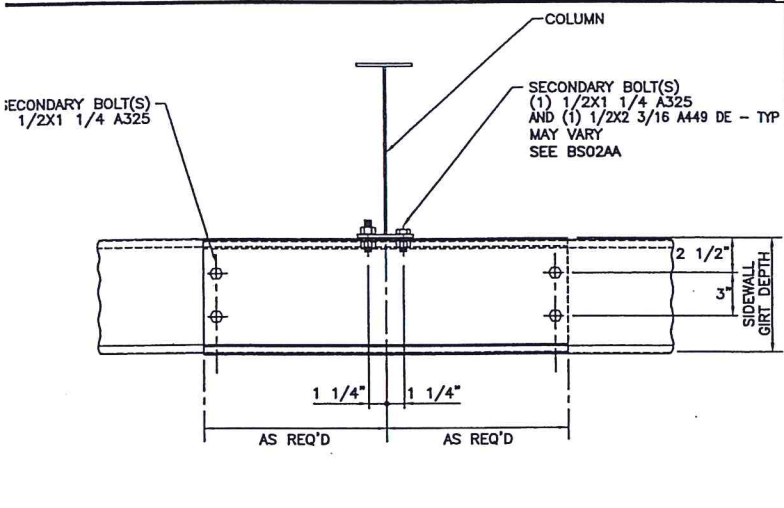
| COLUMN FLANGE THICKNESS | SECONDARY FRAMING GAUGE | DOUBLE ENDED BOLT REQUIRED | 1/2" A325 BOLT REQUIRED FOR LAPPED MEMBERS | 1/2" A325 BOLT REQUIRED FOR NON LAPPED MEMBERS |
|-------------------------|-------------------------|----------------------------|--|--|
| < 3/8" | 11 - 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 15 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 14 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| | 13 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/4 A325 | 1/2 X 1 1/4 A325 |
| 3/8" | 12 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
| | 11 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
| | 16 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/4 A325 |
| | 15 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 14 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| 1/2" | 13 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 12 | 1/2 X 2 3/16 A449 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 11 | 1/2 X 3 5/8 A307 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 18 | 1/2 X 3 5/8 A307 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 15 | 1/2 X 3 5/8 A307 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| 5/8" | 14 | 1/2 X 3 5/8 A307 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 13 | 1/2 X 3 5/8 A307 DE | 1/2 X 1 1/2 A325 | 1/2 X 1 1/2 A325 |
| | 12 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 1 1/2 A325 |
| | 11 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 1 1/2 A325 |
| | 16 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 1 1/2 A325 |
| 3/4" | 15 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| | 14 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| | 13 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| | 12 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| | 11 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| 1" | 11 - 16 | 1/2 X 3 5/8 A307 DE | 1/2 X 2 A325 | 1/2 X 2 A325 |
| 1 1/2" | 11 - 16 | 1/2 X 3 5/8 A307 DE | SPECIAL ORDER | SPECIAL ORDER |

BOLT REQUIREMENTS FOR SECONDARY FRAMING CONNECTIONS AT COLUMNS

BS02
AA

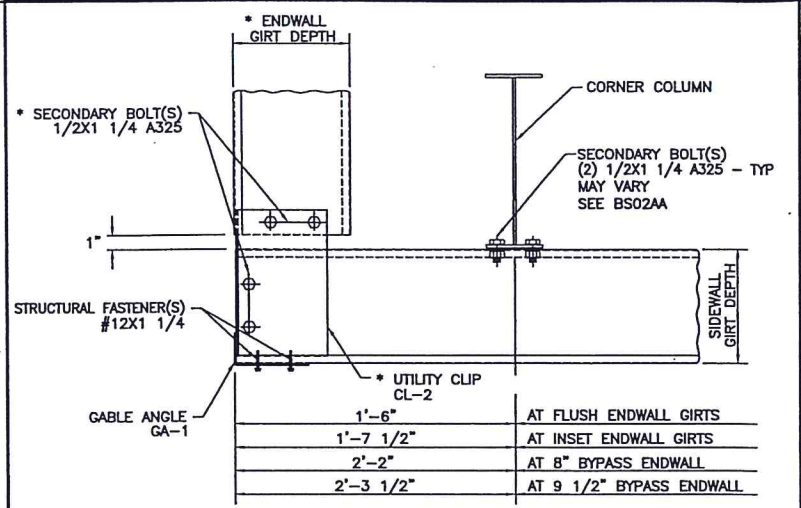


| REVISIONS | | | | | | | REVISIONS | | | | | | | DRAWING SUBMITTAL STATUS | | | SCALE: | | NONE | | DATE | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | | EL PASO | | ILLINOIS | |
|-----------|--|------|------|------|------|------|-----------|--|------|------|------|------|-----------------|--|-------------------|--|-----------|--|------|--|---|--|--|--|--|--|--------------------|------------------|--------------|--|
| NO. | | MADE | CK'D | DATE | ENGR | DATE | NO. | | MADE | CK'D | DATE | ENGR | DATE | [*] FOR CONSTRUCTION [] FOR APPROVAL [] FOR PERMIT ONLY [] FOR PRELIMINARY | DRAWN BY: BJC | | | | | |  AMERICAN BUILDINGS COMPANY | | | | | | DRAWING: NUMBER | 61874601 SED-006 | REV.NO. 0 | |
| | | | | | | | | | | | | | CHECKED BY: | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | DESIGN APPD BY: | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | CAD BY: | | 08/28/08 12:28:21 | | Ver. 30.1 | | | | | | | | | | | | | |



BYPASS LAPPED GIRTS CONNECTION TO COLUMN

WF02R
AA

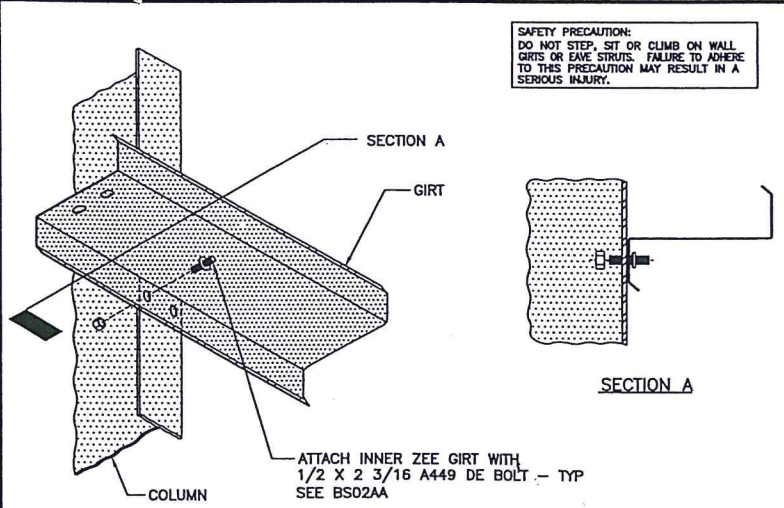


CORNER GIRTS CONNECTION (RIGID FRAME CORNER COLUMN)

WF04
AA

* NOT REQUIRED IF OPEN ENDWALL

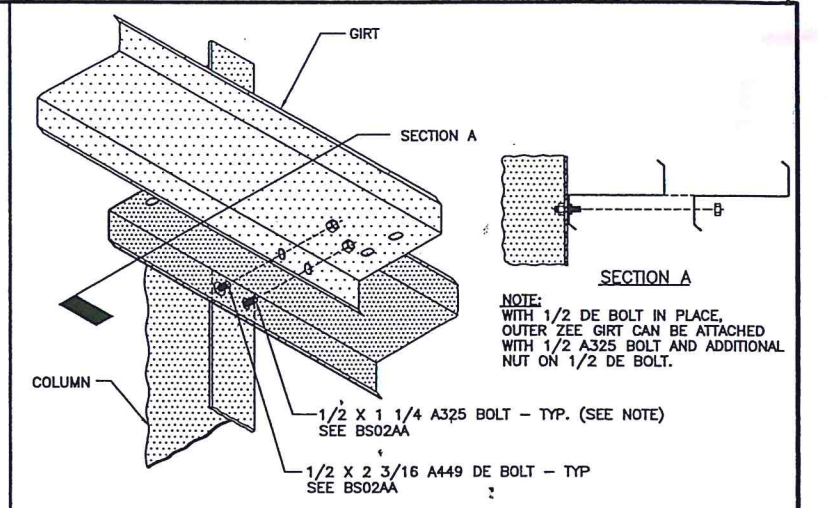
BYPASS SIDEWALL / BYPASS, INSET OR FLUSH ENDWALL



BYPASS GIRTS TO COLUMN CONNECTION (LAPPED GIRTS ONLY)

WF10R
AA

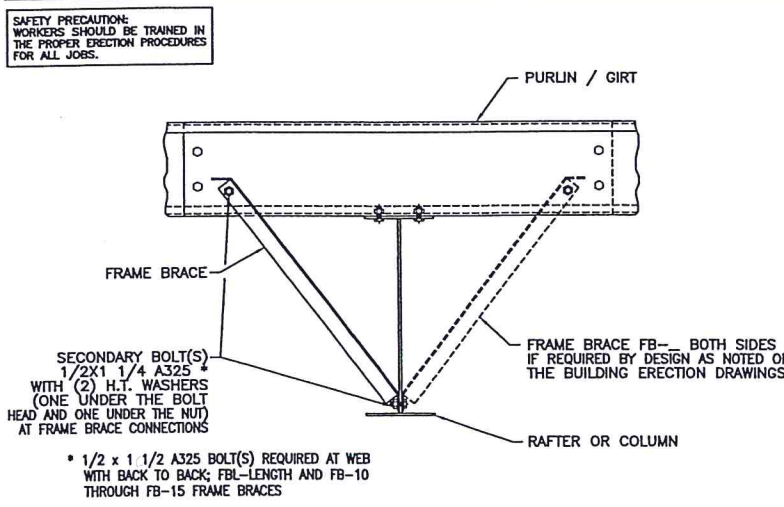
STEP ONE



BYPASS GIRTS TO COLUMN CONNECTION (LAPPED GIRTS ONLY)

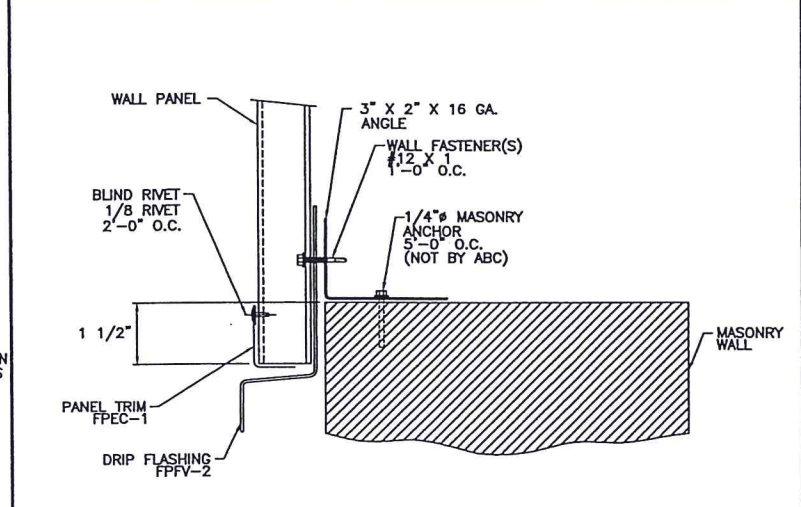
WF10RA
AA

STEP TWO



FRAME BRACE FOR BYPASS PURLINS / GIRTS

BR17
AA



WALL PANEL TO MASONRY DETAIL

MA09
AA

MASONRY APPLICATIONS PARTIAL MASONRY WALLS

| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | SCALE: | NONE | DATE | CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
|-----|-----------|------|------|------|------|------|-----|-----------|------|------|------|------|------|--------------------------|-------------------|-----------|------|----------------------------------|-----------|----------|
| | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | | | | 3129 MAIN | | |
| | | | | | | | | | | | | | | [] FOR APPROVAL | DRAWN BY: BJC | | | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | [] FOR PERMIT ONLY | CHECKED BY: | | | HORIZON CONSTRUCTION | | |
| | | | | | | | | | | | | | | [] FOR PRELIMINARY | DESIGN APPD BY: | | | | | |
| | | | | | | | | | | | | | | CAD BY: | 08/28/08 12:28:22 | Ver. 30.1 | | DRAWING NUMBER: 61874601 SED-007 | REV.NO. 0 | |



EA37
AC



FL08
AC

-
- A line drawing showing a hand and forearm held in a plaster cast. The cast is a long, rectangular block with a textured surface, likely representing plaster. The hand is positioned with the palm facing the viewer, and the fingers are slightly curled. The forearm is visible, extending from the wrist towards the elbow. The cast is shown in a perspective view, with the hand and forearm positioned in the center. The background is plain white.

PANEL INSTALLATION NOTES

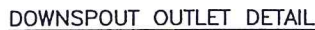
LONGSPAN WALL PANELS

| |
|------|
| GE04 |
| AR |

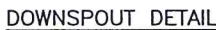
-

PANEL INSTALLATION NOTES

GE04
AC



| |
|------|
| GT01 |
| AA |



| |
|------|
| GT02 |
| AA |

GT03
AA

GT04



| |
|------|
| GT07 |
| AA |




| |
|-------|
| GT18A |
| AA |

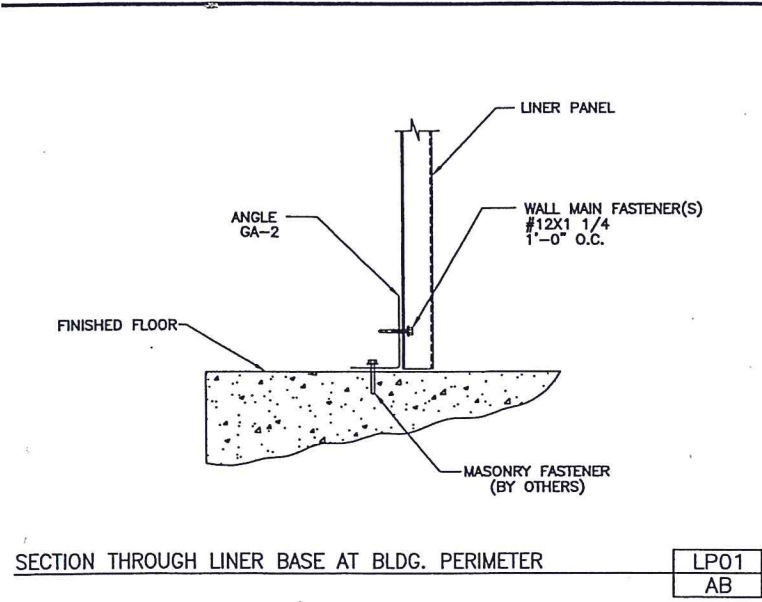
Diagram illustrating the installation of a wall panel. The components shown are:

- INSULATION
- TRIM INSULATION AND TURN VINYL BACK. INSULATION MUST NOT BE EXPOSED TO WEATHER.
- WALL PANEL
- BASE ANGLE

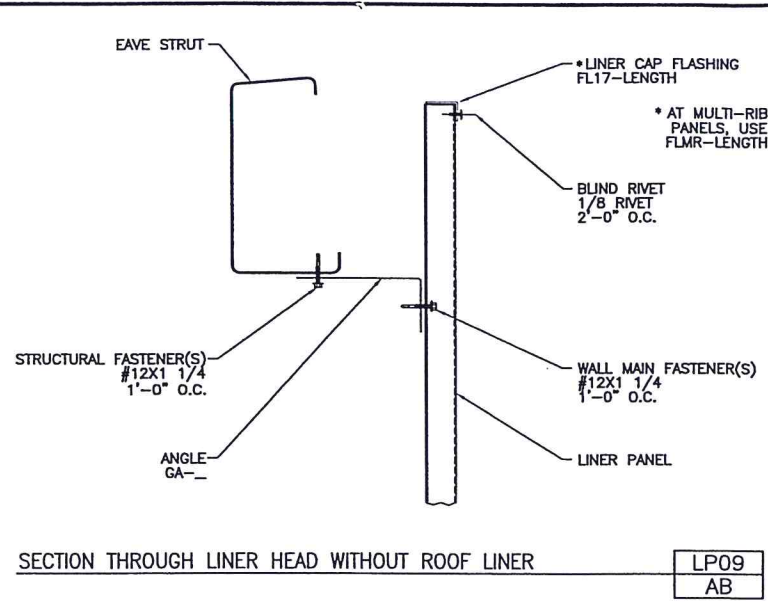
SUGGESTED INSULATION DETAIL AT WALL PANEL BASE

| |
|------|
| IN02 |
|------|

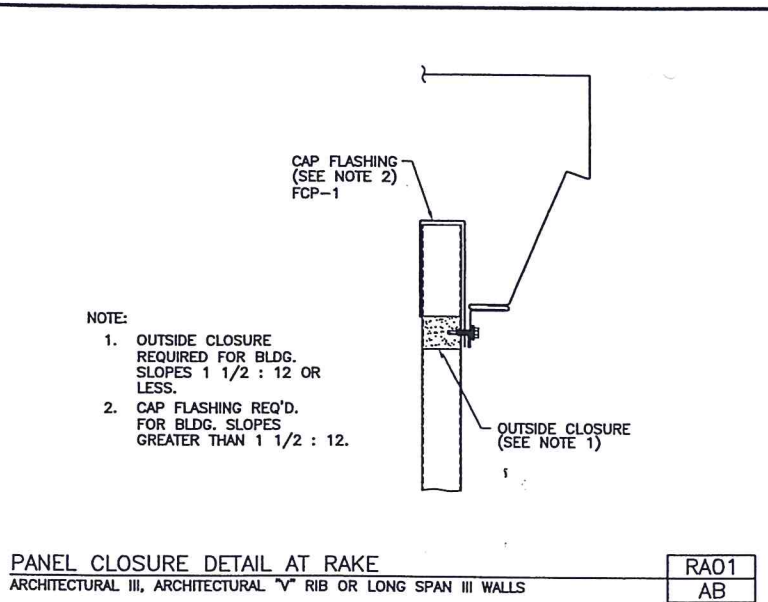
| REVISIONS | | | | | | REVISIONS | | | | | | REVISIONS | | | | | | DRAWING SUBMITTAL STATUS | | SCALE: | | NONE | | DATE | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION |  AMERICAN BUILDINGS COMPANY | EL PASO ILLINOIS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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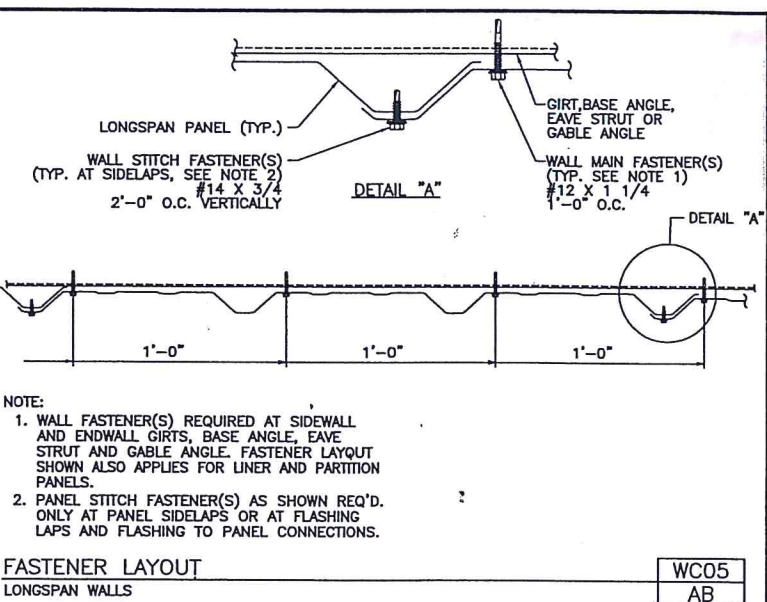
SECTION THROUGH LINER BASE AT BLDG. PERIMETER
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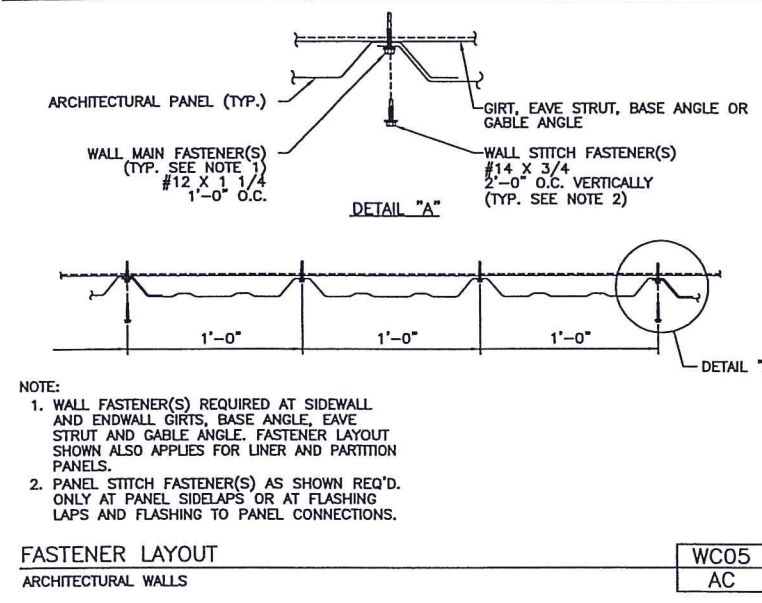
SECTION THROUGH LINER HEAD WITHOUT ROOF LINER
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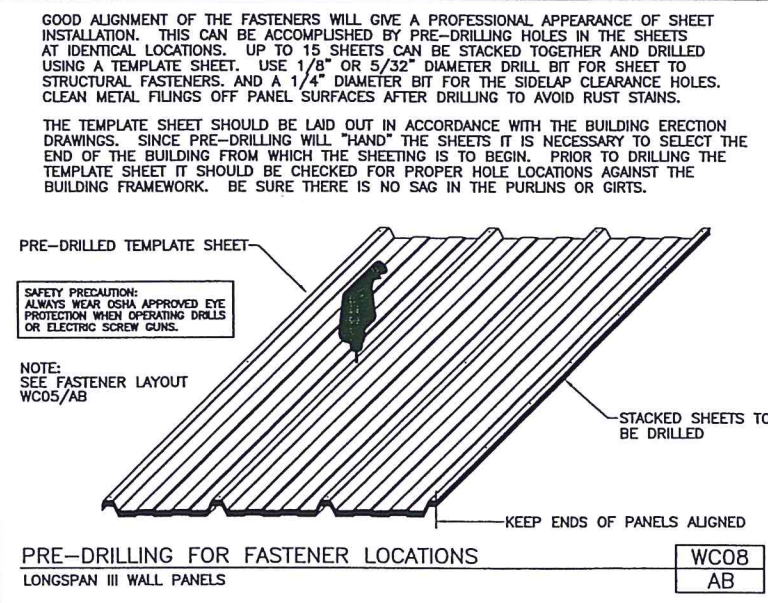
PANEL CLOSURE DETAIL AT RAKE
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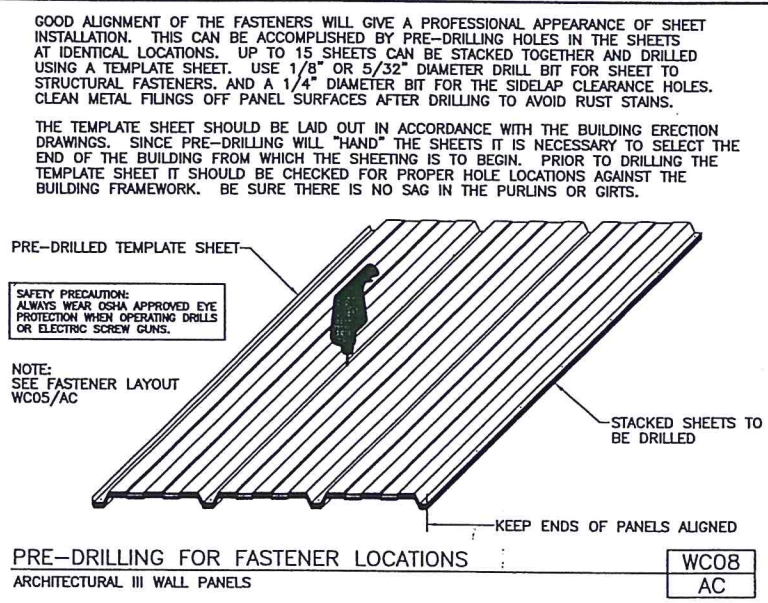
FASTENER LAYOUT
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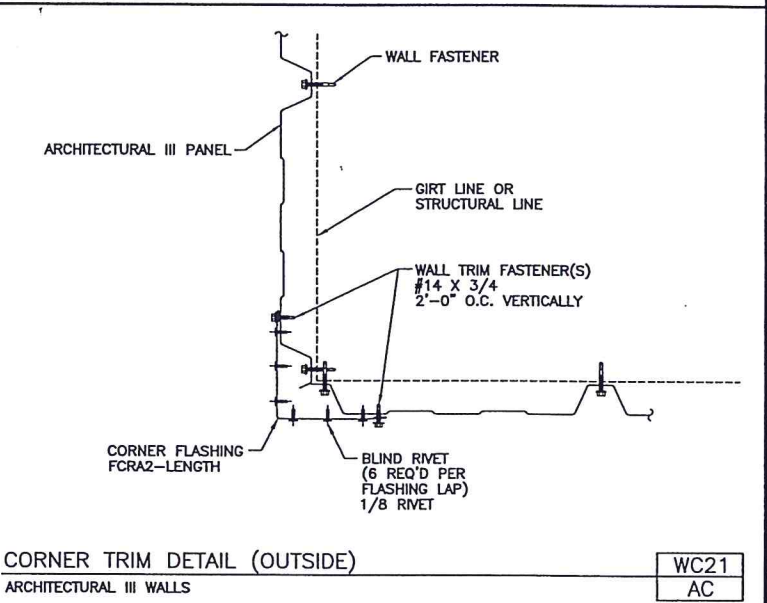
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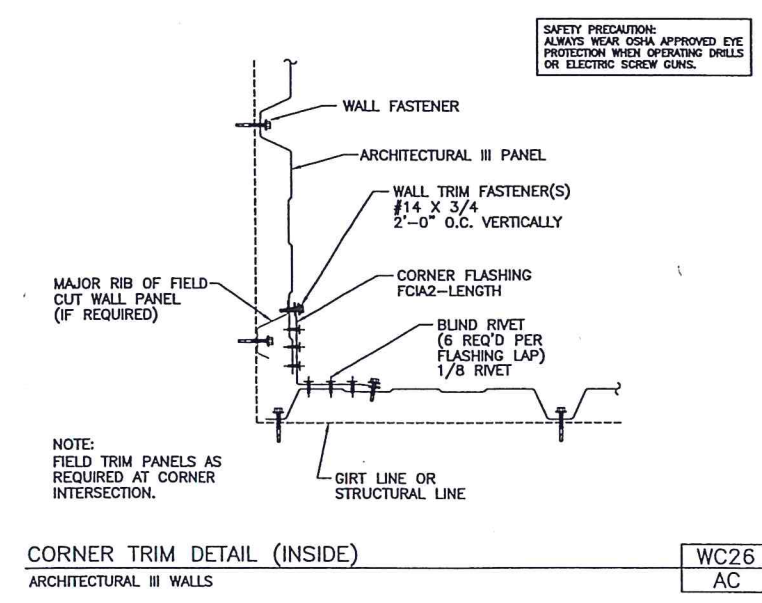
PRE-DRILLING FOR FASTENER LOCATIONS
LONGSPAN III WALL PANELS
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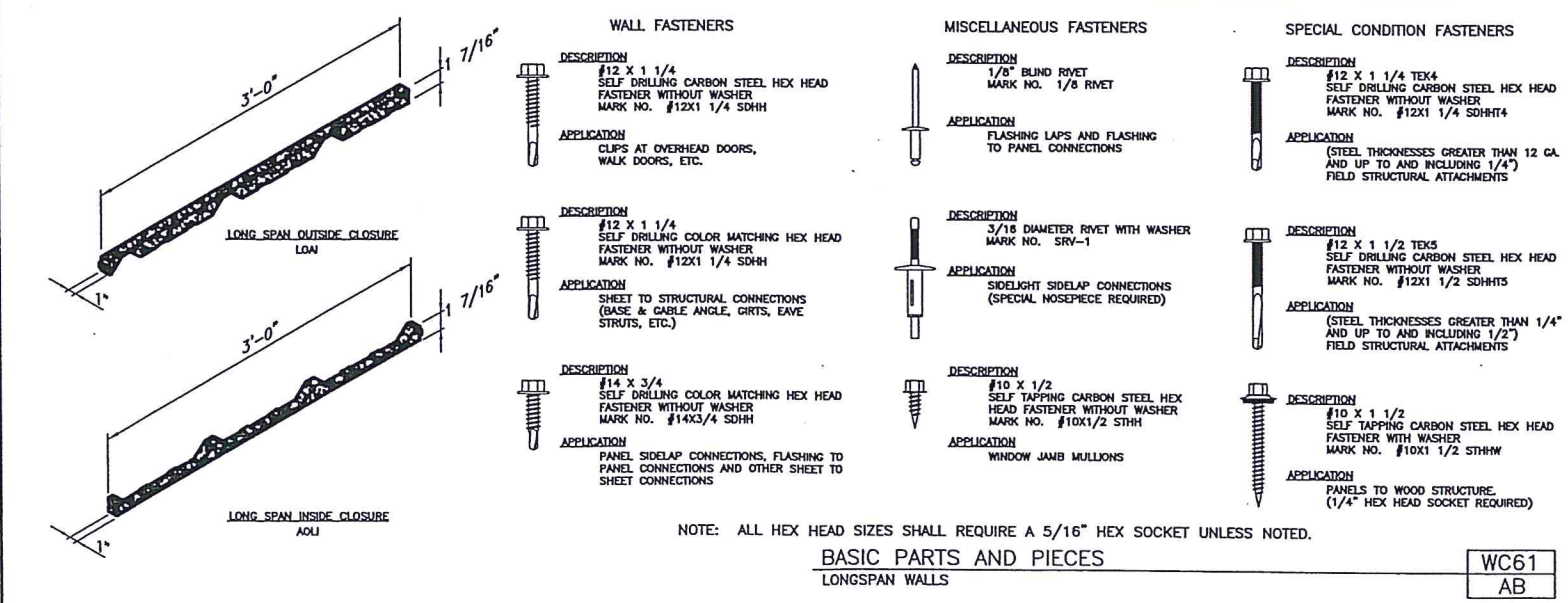
PRE-DRILLING FOR FASTENER LOCATIONS
ARCHITECTURAL III WALL PANELS
WC08
AC



CORNER TRIM DETAIL (OUTSIDE)
ARCHITECTURAL III WALLS
WC21
AC



CORNER TRIM DETAIL (INSIDE)
ARCHITECTURAL III WALLS
WC26
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BASIC PARTS AND PIECES
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| REVISIONS | | | | | | | REVISIONS | | | | | | | DRAWING SUBMITTAL STATUS | | | SCALE: NONE DATE | | | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | EL PASO ILLINOIS | |
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| NO. | MADE | CK'D | DATE | ENGR | DATE | NO. | MADE | CK'D | DATE | ENGR | DATE | [*] FOR CONSTRUCTION [] FOR APPROVAL [] FOR PERMIT ONLY [] FOR PRELIMINARY | | | DRAWN BY: BJC CHECKED BY: DESIGN APPD BY: | | | AMERICAN BUILDINGS COMPANY | | | | | DRAWING NUMBER | REV.NO. |
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PANEL BACKLAPPING
ARCHITECTURAL III, ARCHITECTURAL V RIB OR LONG SPAN III WALLS

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| WC75 |
| AB |

1. ALL AREAS WHERE MASTIC IS TO BE APPLIED SHOULD BE WIPED CLEAN WITH A MILD DETERGENT OR AN ALL PURPOSE CLEANER BEFORE MASTIC APPLICATION. THIS WILL INSURE A GOOD SEALING SURFACE AND IMPROVE WEATHER TIGHTNESS.
2. THE BLANKET INSULATION MANUFACTURER RECOMMENDS THAT DOUBLE SIDED TAPE BE USED TO SECURE THE INSULATION TO THE EAVE. AMERICAN BUILDINGS COMPANY IS NOT RESPONSIBLE FOR THE INSTALLATION OR ATTACHMENT OF THE INSULATION. ADDITIONAL ATTACHMENTS (NOT BY A B C) MAY BE REQUIRED.
3. ALL EXPOSED FASTENERS SHOULD PENETRATE THE SEALANT FOR THE MOST WEATHER TIGHT CONNECTION IN WALL AREAS WHERE APPLICABLE.
4. THE ARCHITECTURAL PANEL IS DESIGNED FOR WALL APPLICATIONS ONLY AND SHOULD NEVER BE UTILIZED IN THE ROOF FOR PURPOSES OF PROVIDING A ROOF SYSTEM FOR BUILDINGS. THE INVERTED RIBS INCORPORATED INTO ITS DESIGN PRODUCE SMOOTH SHADOW LINES AND SEMI-CONCEALED FASTENERS. ALTHOUGH SHEETING CAN USUALLY BEGIN FROM EITHER END OF THE BUILDING, APPLICATION OF THIS PANEL IS DIRECTIONAL. PROPERLY INSTALLED, THE LAP EDGES OF THE ARCHITECTURAL PANEL HAVE MINIMUM VISIBILITY.
5. WHEN FIELD CUTTING PANELS OR TRIM DO NOT USE ABRASIVE SAWS OR OTHER CUTTING METHODS WHICH PRODUCE HOT METAL PARTICLES OR BURN THE CUT EDGES. THESE METHODS WILL DAMAGE THE PAINTED AND GALVALUME FINISH AND VOID ANY WARRANTIES. USE DOUBLE CUT SHEARS, NIBBLERS OR OTHER CUTTING DEVICES WHICH DO NOT PRODUCE HOT METAL PARTICLES OR BURNED EDGES.

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| WC91 |
| AC |

SAFETY PRECAUTION:
WORKERS SHOULD ALWAYS WEAR
GLOVES WHEN HANDLING PANELS.

LONGSPAN III PANELS SHOWN,
ARCHITECTURAL III AND
MULTI RIB SIMILAR.

TARP OR PLASTIC COVER

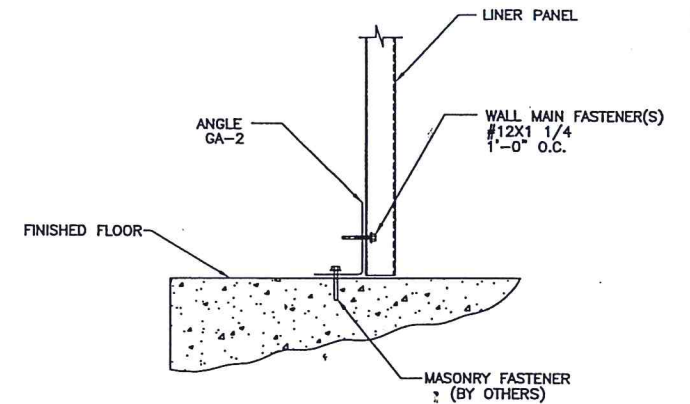
USE SPACERS BETWEEN BUNDLES

BLOCK ABOVE GROUND

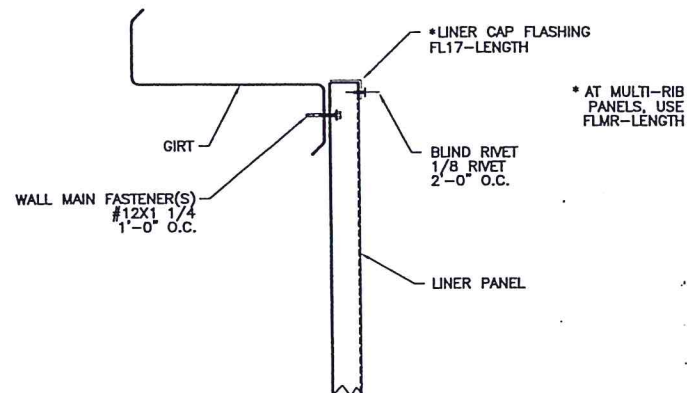
TIE DOWN

LONG SPAN III, ARCHITECTURAL III, AND MULTI-RIB PANELS

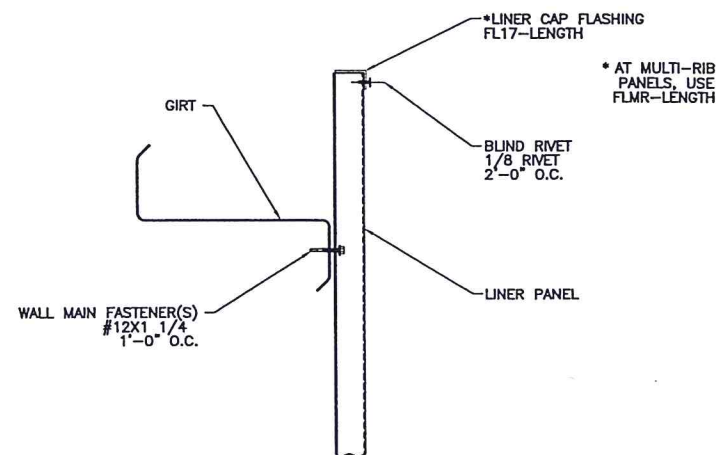
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| GE51 |
| AA |



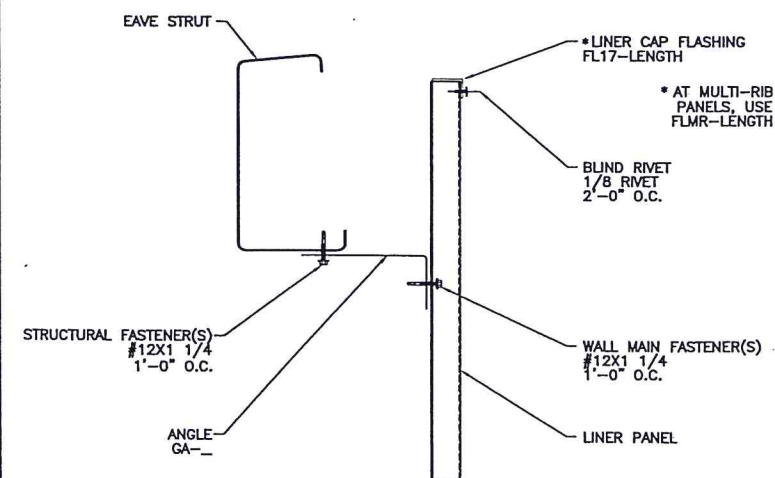
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| LP01 |
| AR |



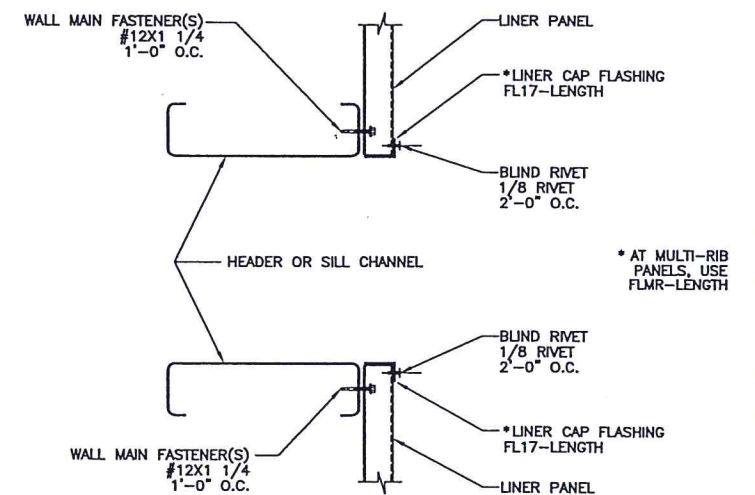
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| LP06 |
| AB |



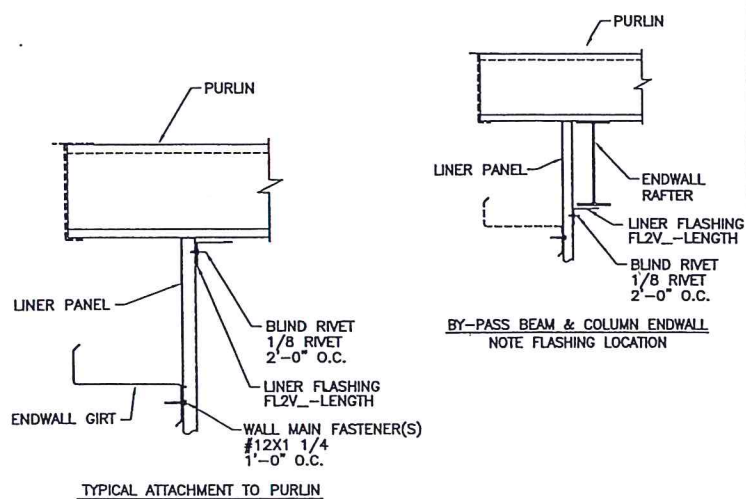
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| LP07 |
| AB |




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| LP09 |
| AB |

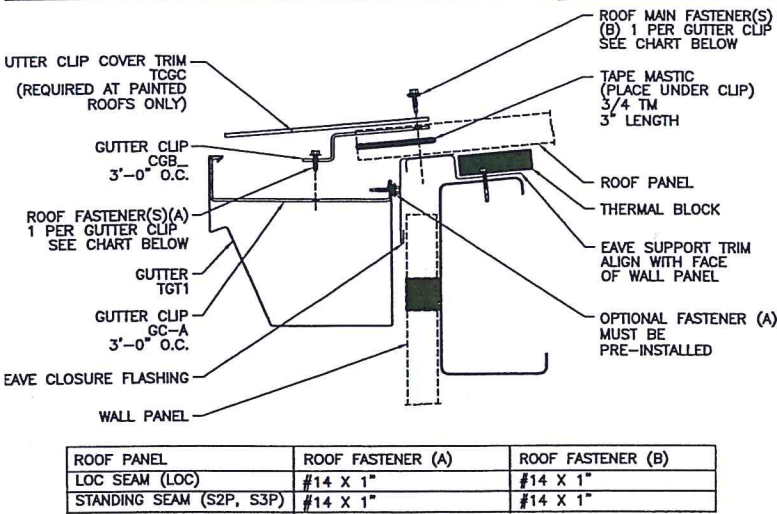


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| LP52 |
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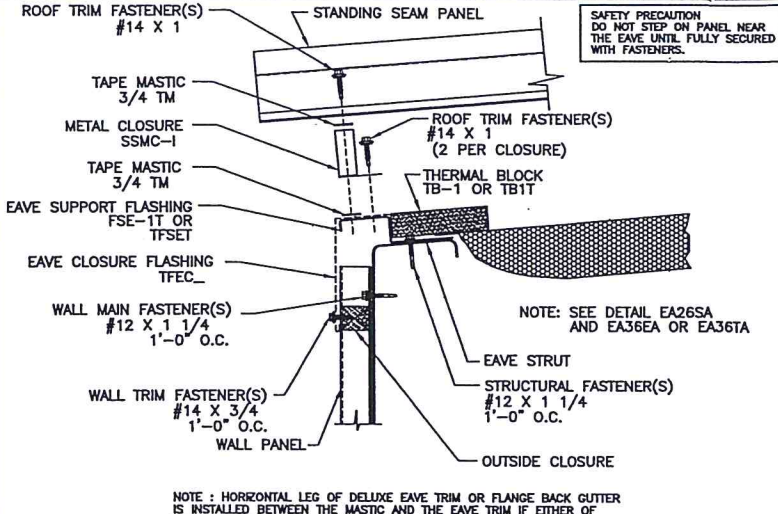


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| LP17 |
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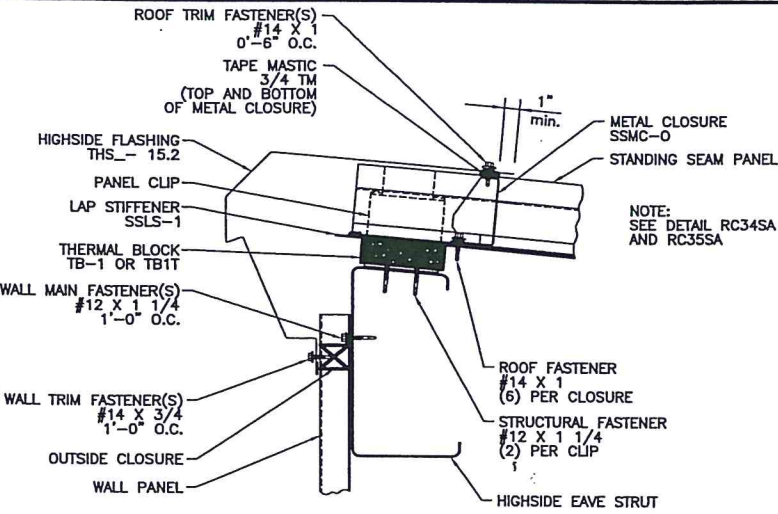
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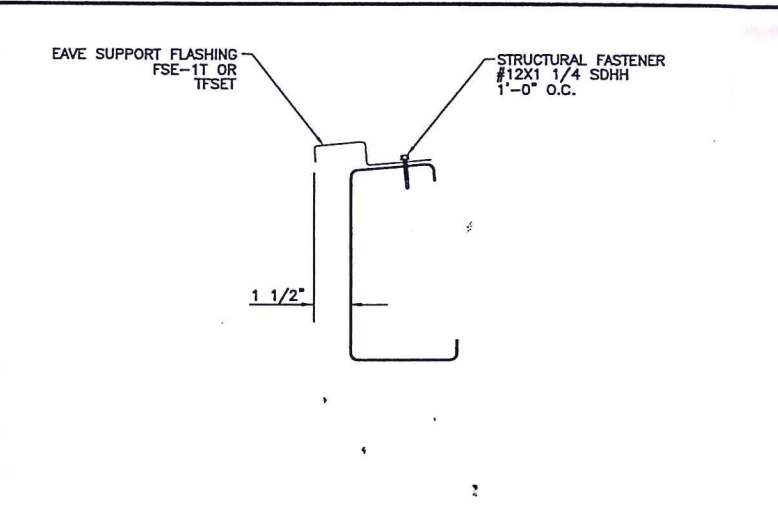
STANDARD GUTTER DETAIL WITH GUTTER CLIP ATTACHMENT WITH THERMAL BLOCKS



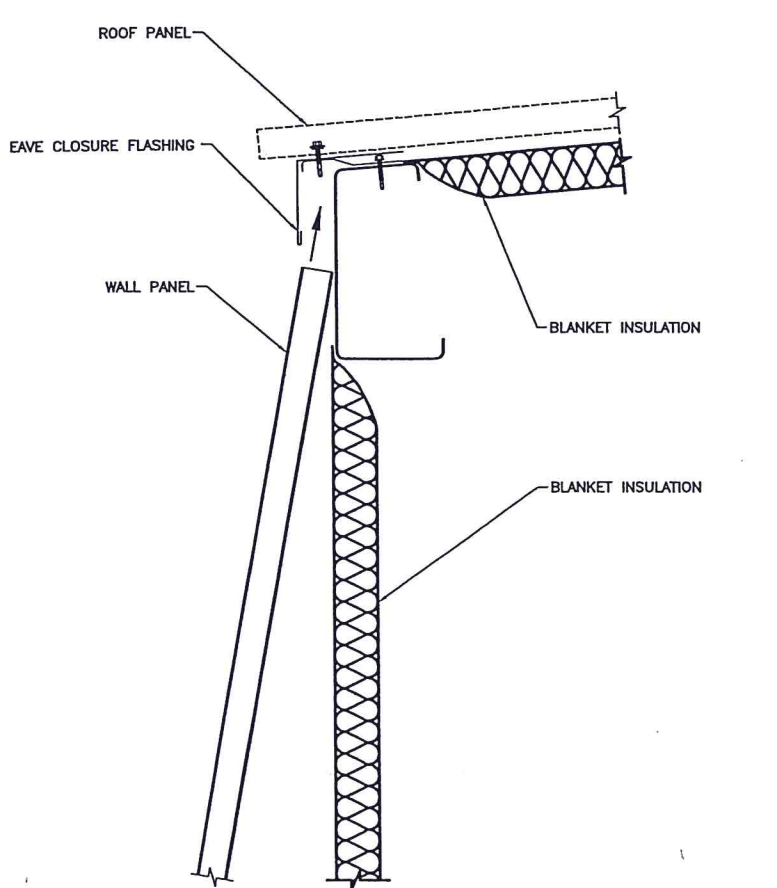
SECTION AT EAVE WITH THERMAL BLOCK



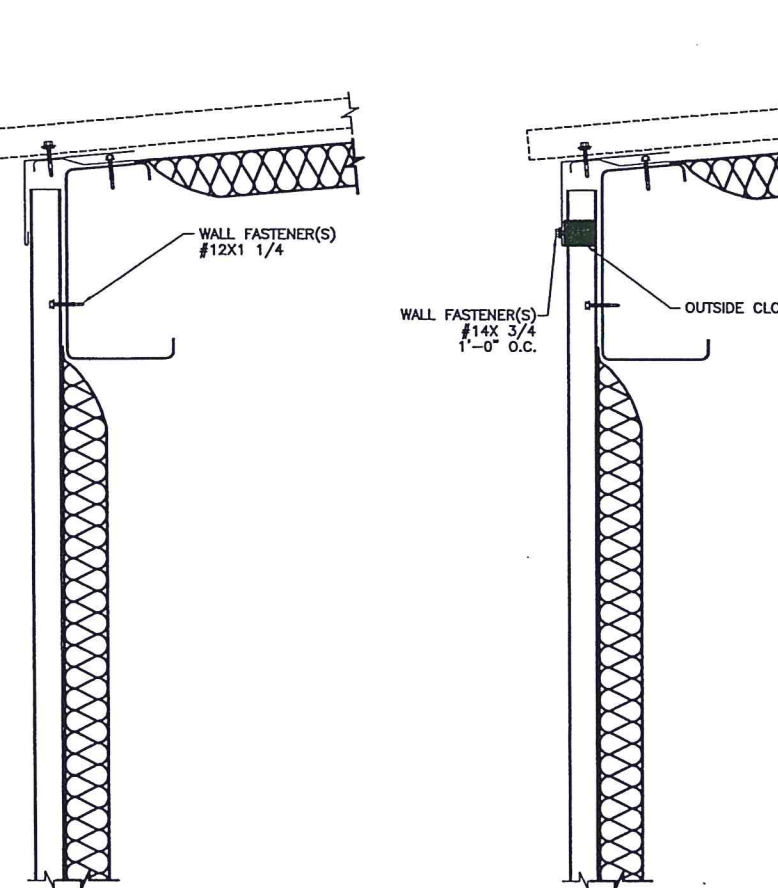
HIGHSIDE EAVE FLASHING ATTACHMENT WITH THERMAL BLOCK



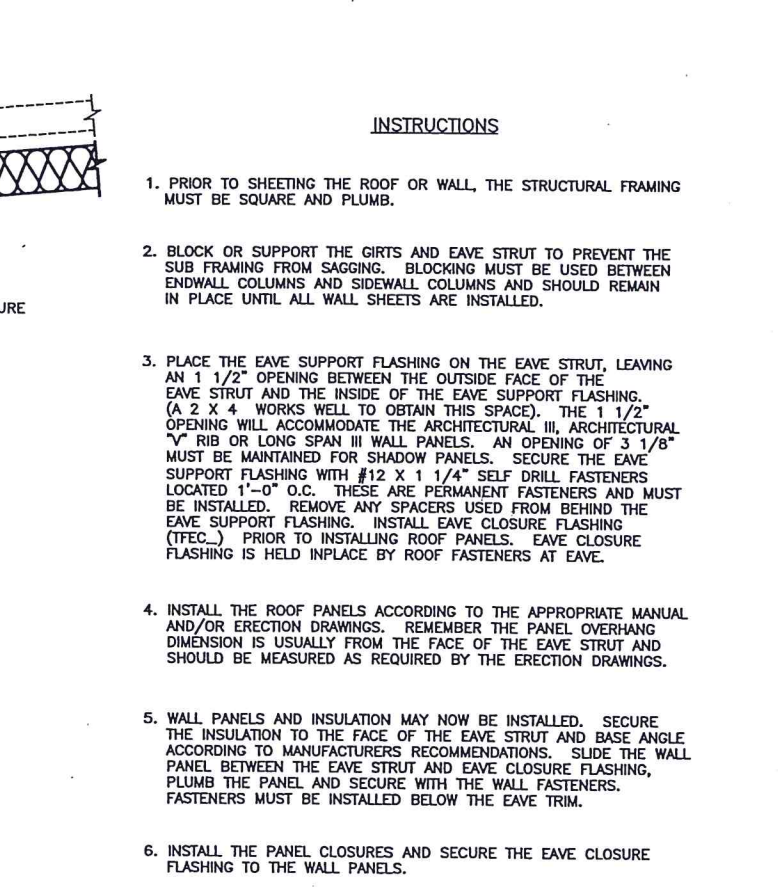
THERMAL EAVE SUPPORT FLASHING ATTACHMENT



STEP 1. SEE INSTRUCTION NO. 5

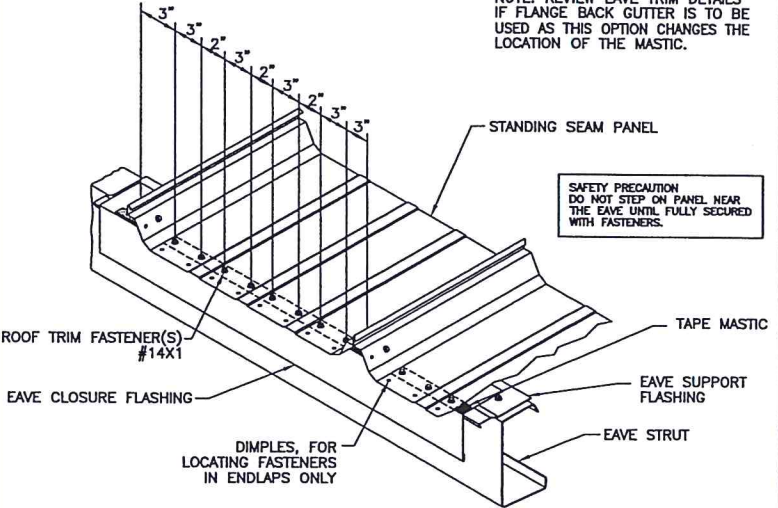


STEP 2. SEE INSTRUCTION NO. 5



STEP 3. SEE INSTRUCTION NO. 6

- INSTRUCTIONS
1. PRIOR TO SHEETING THE ROOF OR WALL, THE STRUCTURAL FRAMING MUST BE SQUARE AND PLUMB.
 2. BLOCK OR SUPPORT THE GIRTS AND EAVE STRUT TO PREVENT THE SUB FRAMING FROM SAGGING. BLOCKING MUST BE USED BETWEEN ENDWALL COLUMNS AND SIDEWALL COLUMNS AND SHOULD REMAIN IN PLACE UNTIL ALL WALL SHEETS ARE INSTALLED.
 3. PLACE THE EAVE SUPPORT FLASHING ON THE EAVE STRUT, LEAVING AN 1 1/2" OPENING BETWEEN THE OUTSIDE FACE OF THE EAVE STRUT AND THE INSIDE OF THE EAVE SUPPORT FLASHING. (A 2 X 4 WORKS WELL TO OBTAIN THIS SPACE). THE 1 1/2" OPENING WILL ACCOMMODATE THE ARCHITECTURAL III, ARCHITECTURAL "V" RIB OR LONG SPAN III WALL PANELS. AN OPENING OF 3 1/8" MUST BE MAINTAINED FOR SHADOW PANELS. SECURE THE EAVE SUPPORT FLASHING WITH #12 X 1 1/4" SELF DRILL FASTENERS LOCATED 1'-0" O.C. THESE ARE PERMANENT FASTENERS AND MUST BE INSTALLED. REMOVE ANY SPACERS USED FROM BEHIND THE EAVE SUPPORT FLASHING. INSTALL EAVE CLOSURE FLASHING (TFEC) PRIOR TO INSTALLING ROOF PANELS. EAVE CLOSURE FLASHING IS HELD INPLACE BY ROOF FASTENERS AT EAVE.
 4. INSTALL THE ROOF PANELS ACCORDING TO THE APPROPRIATE MANUAL AND/OR ERECTION DRAWINGS. REMEMBER THE PANEL OVERHANG DIMENSION IS USUALLY FROM THE FACE OF THE EAVE STRUT AND SHOULD BE MEASURED AS REQUIRED BY THE ERECTION DRAWINGS.
 5. WALL PANELS AND INSULATION MAY NOW BE INSTALLED. SECURE THE INSULATION TO THE FACE OF THE EAVE STRUT AND BASE ANGLE ACCORDING TO MANUFACTURERS RECOMMENDATIONS. SLIDE THE WALL PANEL BETWEEN THE EAVE STRUT AND EAVE CLOSURE FLASHING, PLUMB THE PANEL AND SECURE WITH THE WALL FASTENERS. FASTENERS MUST BE INSTALLED BELOW THE EAVE TRIM.
 6. INSTALL THE PANEL CLOSURES AND SECURE THE EAVE CLOSURE FLASHING TO THE WALL PANELS.

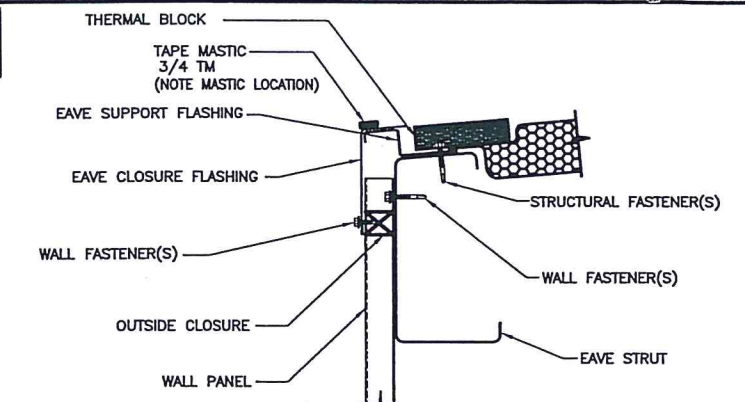
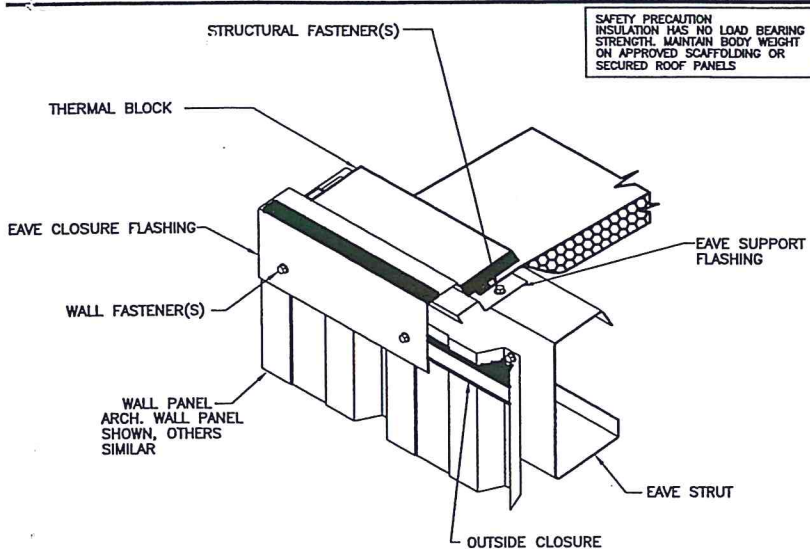


FASTENER INSTALLATION AT EAVE STRUT

NOTE:
SHADOW PANEL INSTALLATION SHALL VARY FROM THE DETAILS SHOWN. SEE THE SHADOW PANEL ERECTION DRAWINGS FOR FASTENER AND FLASHING DETAILS.

ROOF SHEETING PRIOR TO WALL SHEETING INSTRUCTIONS

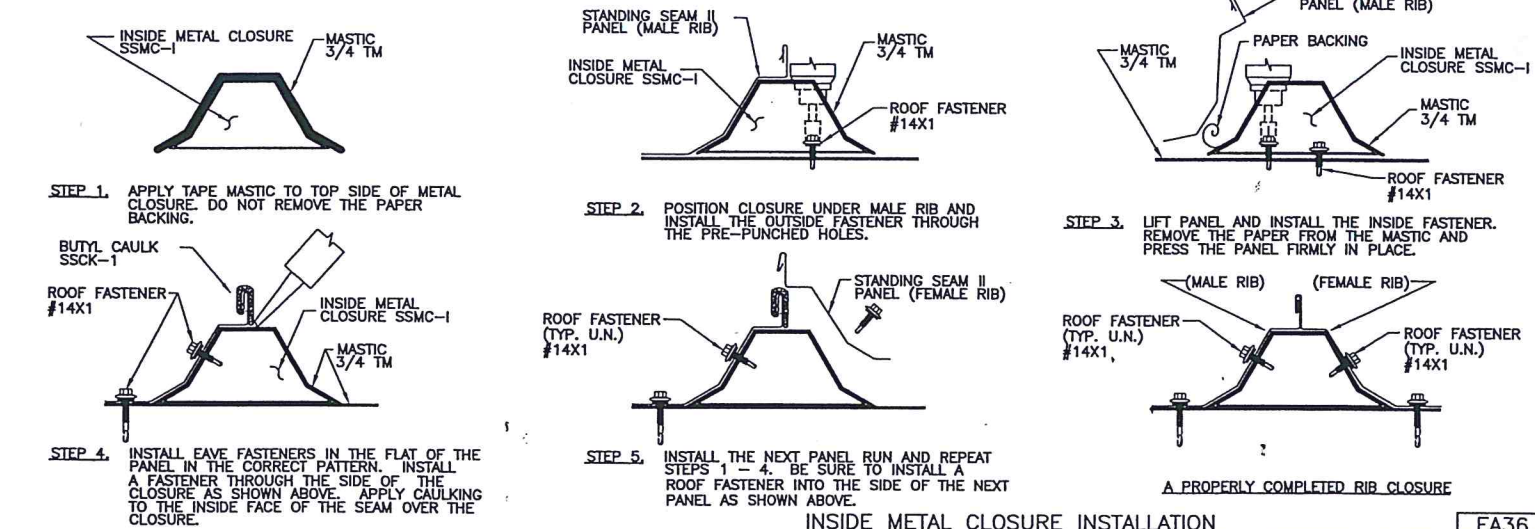
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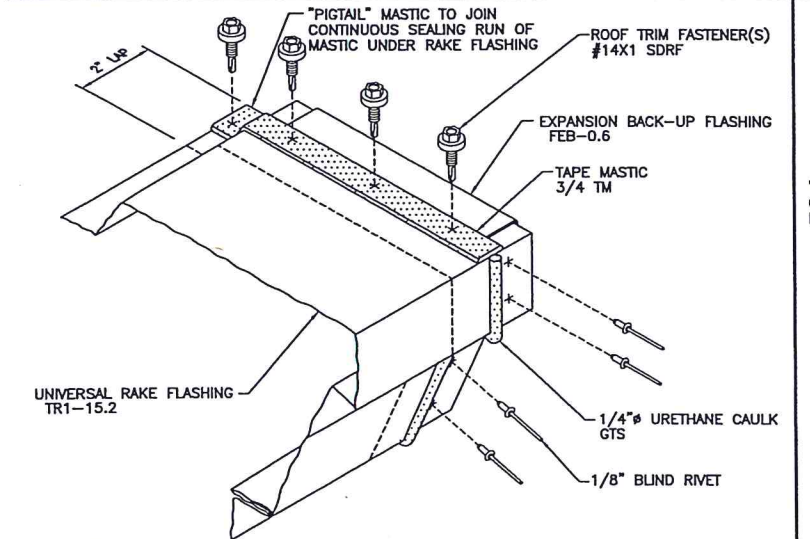
PRIOR TO THE INSTALLATION OF THE FIRST ROOF PANEL, THE SIDEWALL AND ENDWALL SHEETING SHOULD HAVE BEEN COMPLETED AND THE ROOF INSULATION STARTED. START A LINE OF 3/4" X 3/16" TAPE MASTIC DOWN THE EAVES AS SHOWN ABOVE. LEAVE THE PAPER BACKING ON THE TAPE UNTIL THE ROOF PANEL IS READY TO BE SECURED.

MASTIC AND INSULATION APPLICATION AT EAVE WITH THERMAL BLOCKS EA28C AB

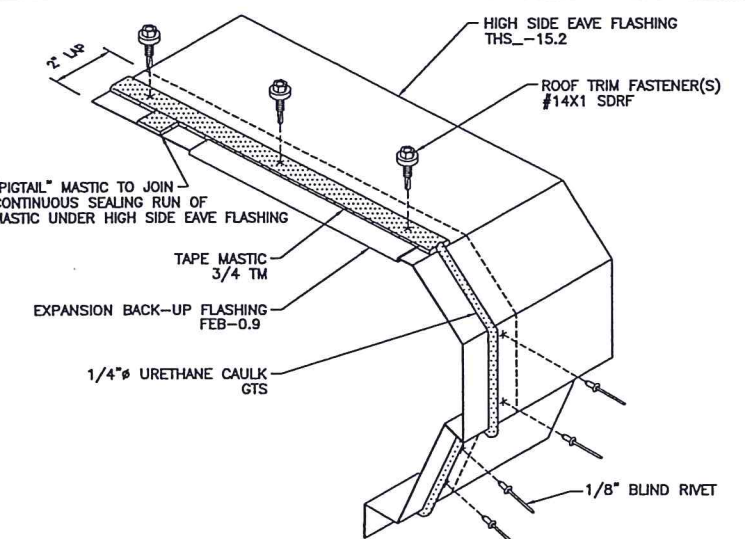
AS YOU ARE SETTING EACH PANEL, INSTALL THE EAVE CLOSURE AS PER THE FOLLOWING INSTRUCTIONS



INSIDE METAL CLOSURE INSTALLATION EA36 EA



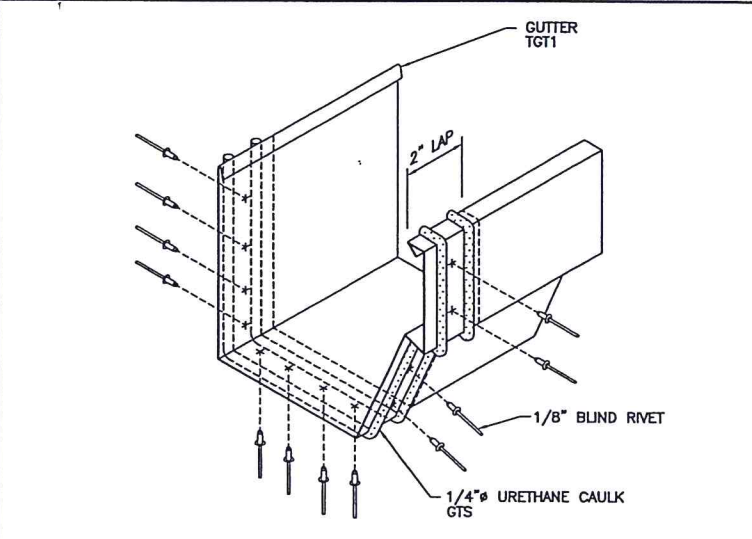
UNIVERSAL RAKE FLASHING FL02 AA



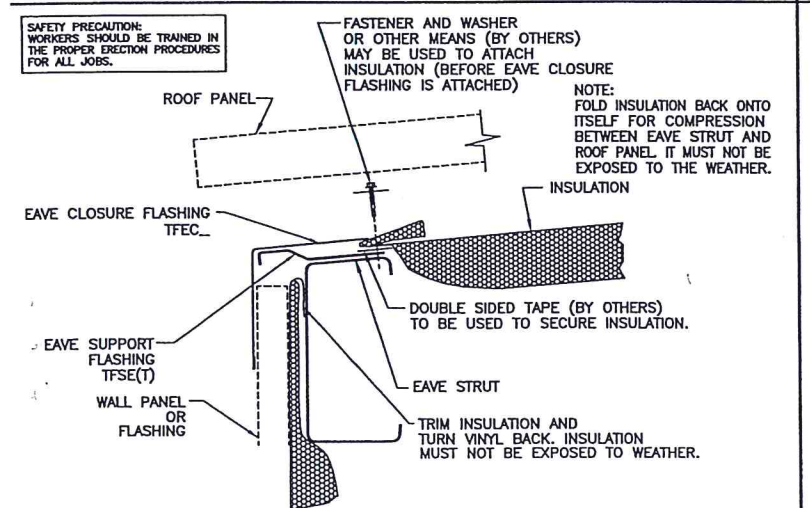
HIGH SIDE EAVE FLASHING FL03 SA

- NOTES:
- ALL ROOF FLASHING LAPS SHALL BE SEALED WITH 3/4 X 3/16 TAPE MASTIC AND REINFORCED USING BACKUP FLASHING FEB- (SEE DETAIL RC35AAA).
 - ALL FLASHING LAPS SHALL BE TWO INCHES EXCEPT CORNER FLASHING LAPS WHICH SHALL BE ONE INCH.
 - VALLEY GUTTER LAPS SHALL BE SEALED WITH 2 X 3/16 TAPE MASTIC AND SHALL BE STITCHED WITH #14 ROOF FASTENERS TWO INCHES ON CENTER. QUANTITY WILL VARY WITH GUTTER SIZE.
 - FOR FLASHING LAPS NOT SHOWN, USE THE FOLLOWING AS A GUIDE; FLASHINGS THAT ARE EXPOSED TO ROOF CONDITIONS, USE #14 ROOF FASTENERS THREE INCHES ON CENTER. ALL OTHER CONDITIONS USE 1/8 INCH BLIND RIVETS THREE INCHES ON CENTER.

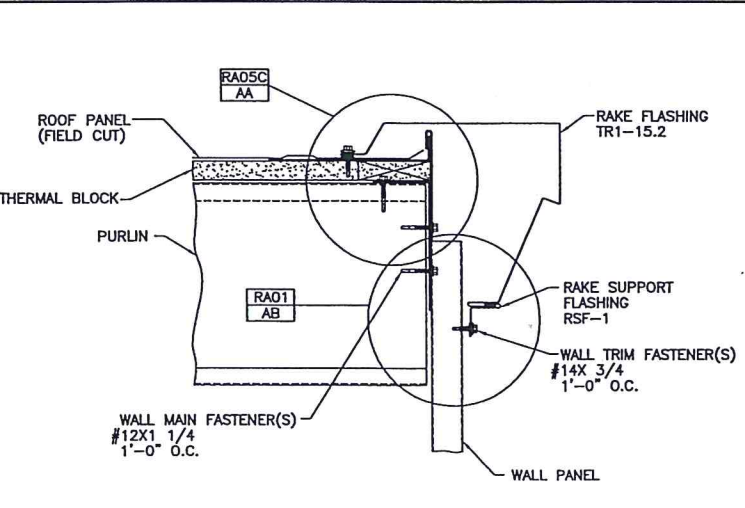
FLASHING LAP GENERAL NOTES FL06 AA



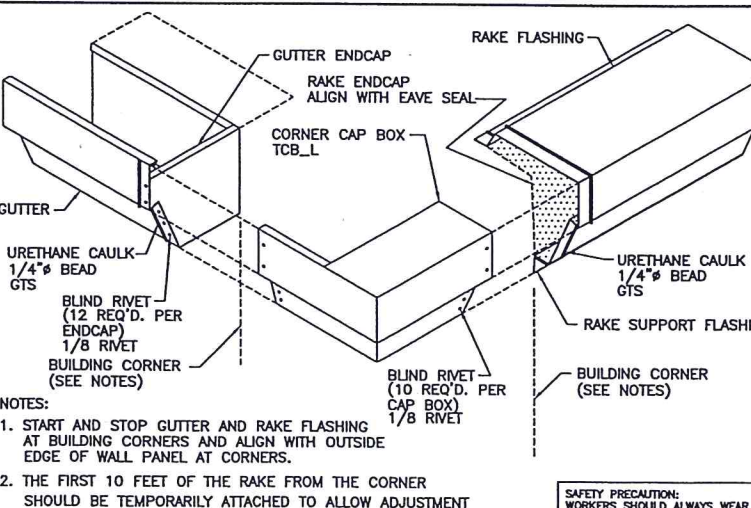
STANDARD GUTTER FL09 AA



SUGGESTED INSULATION ATTACHMENTS AT EAVE IN01 AA



UNIVERSAL RAKE DETAIL RA02C AA

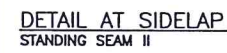
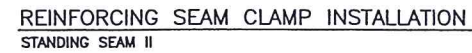
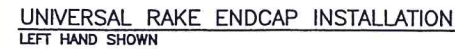



CORNER CAP BOX INSTALLATION RA04 AA

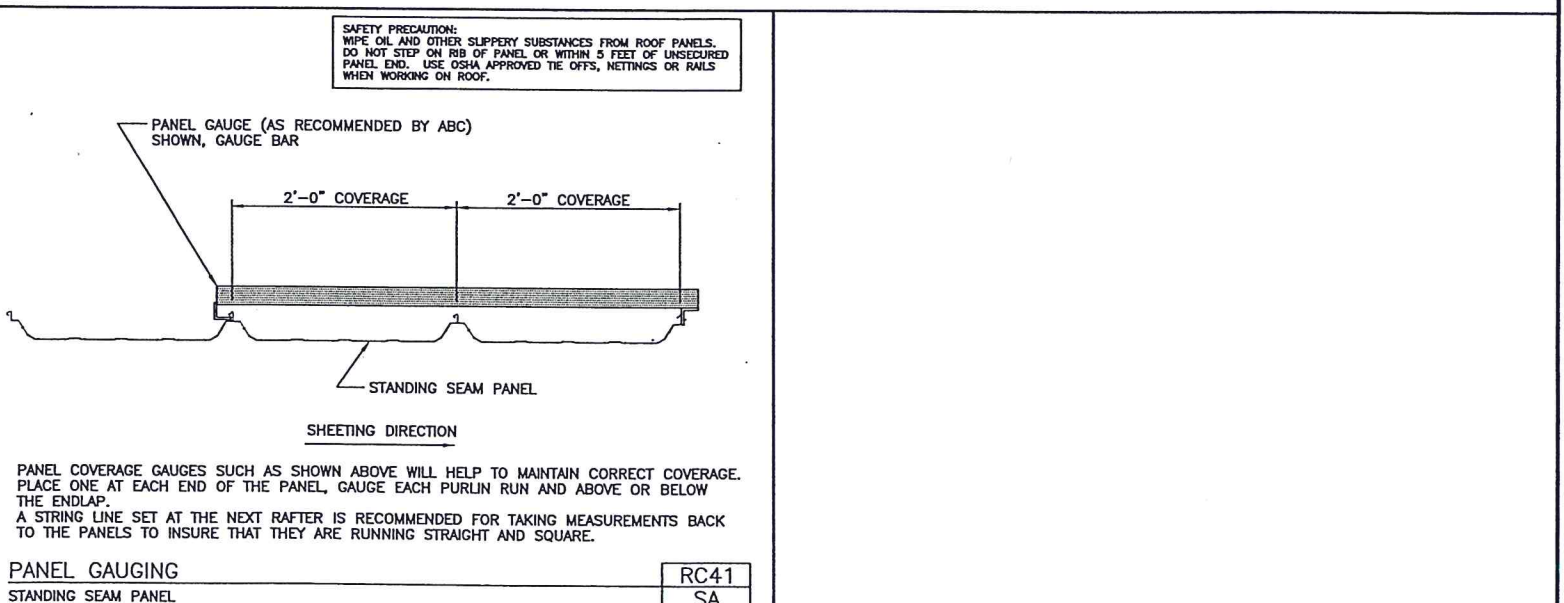
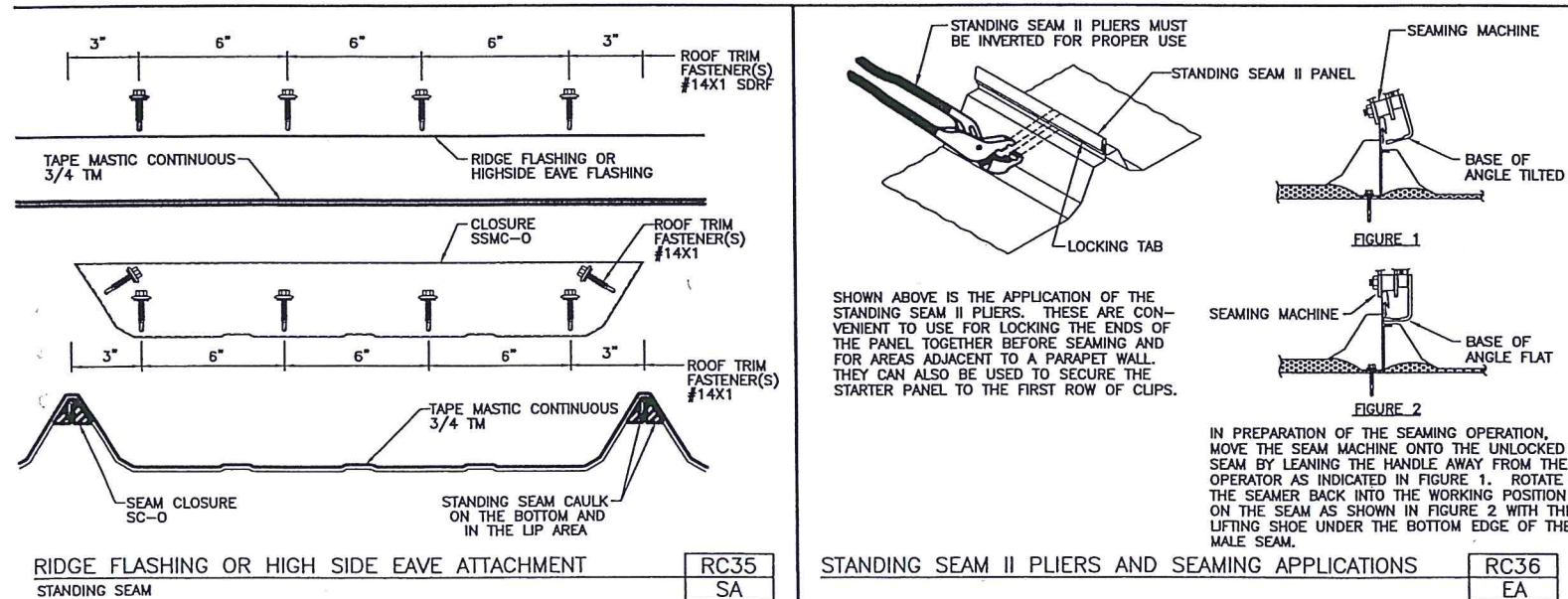
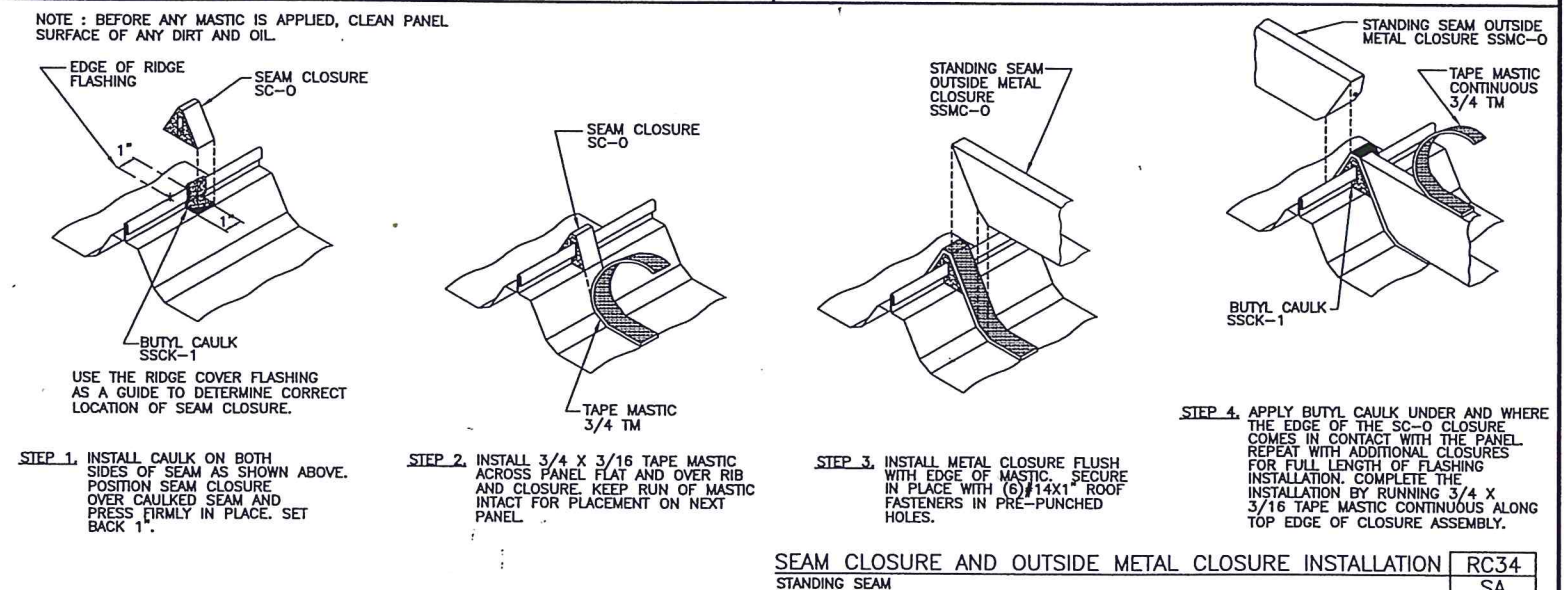
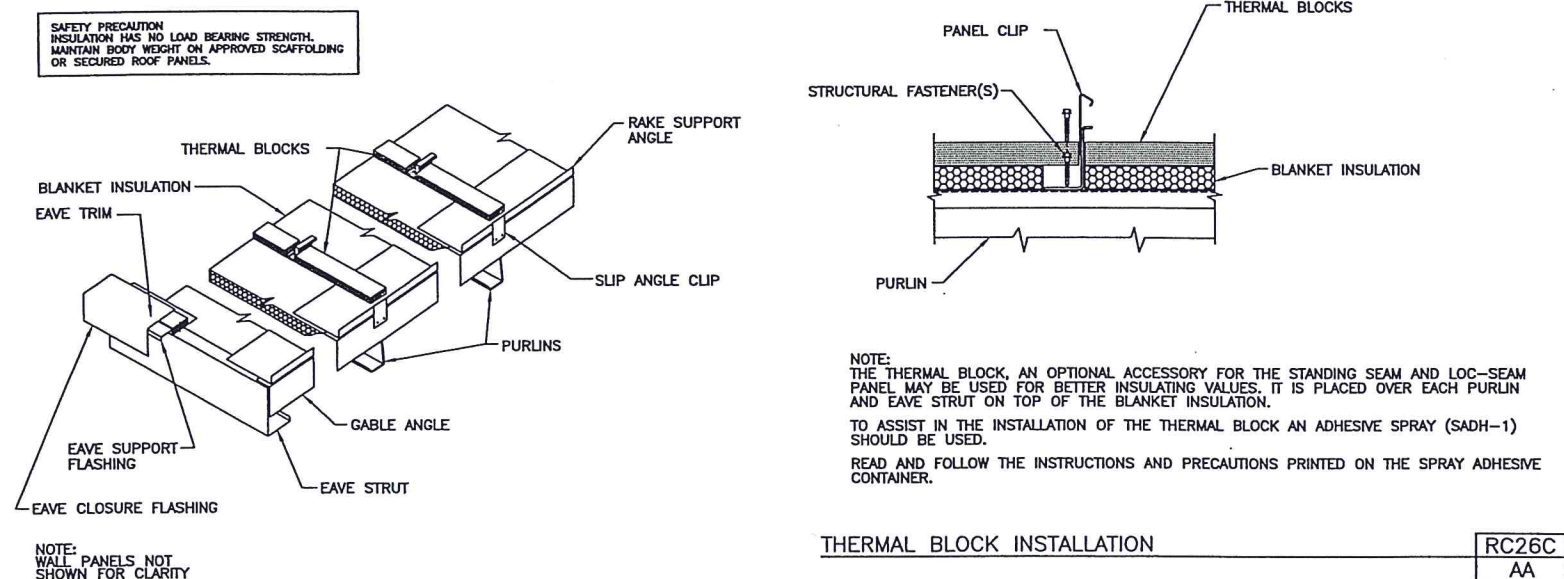
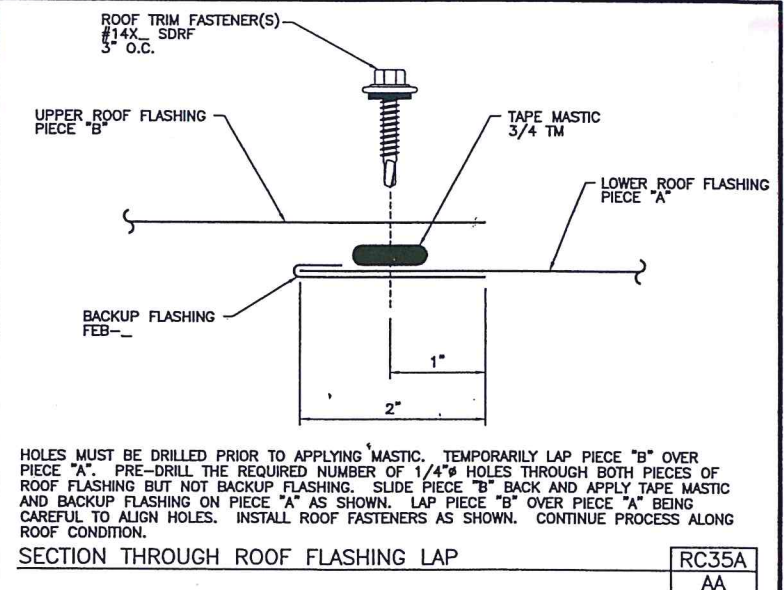
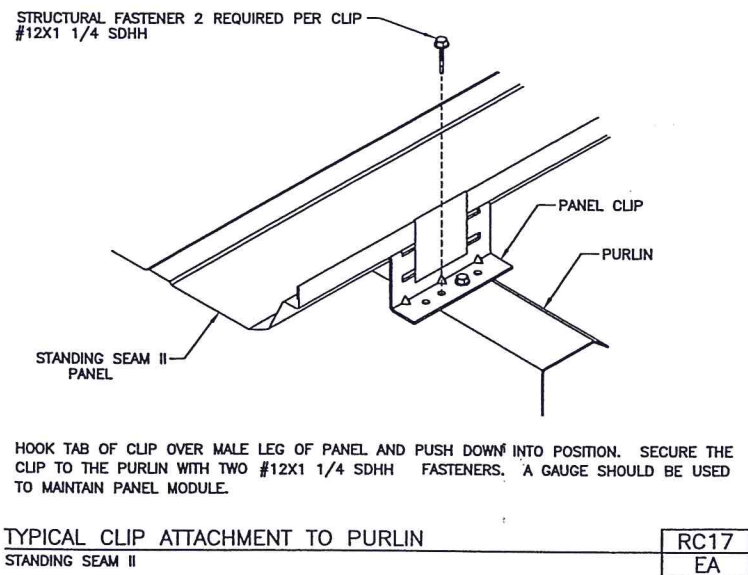
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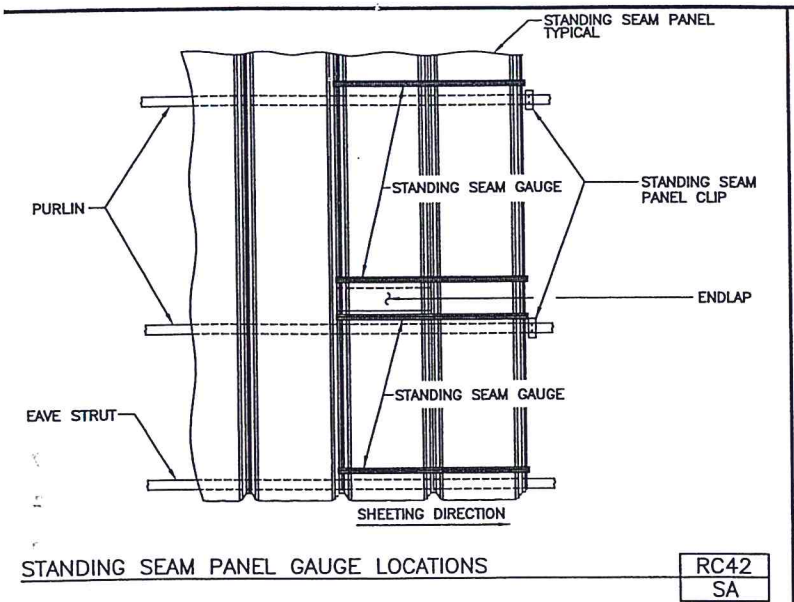
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| CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
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| KANSAS CITY, MO | | |
| HORIZON CONSTRUCTION | | |
| DRAWING NUMBER: 61874601 SED-013 | REV. NO. 0 | |



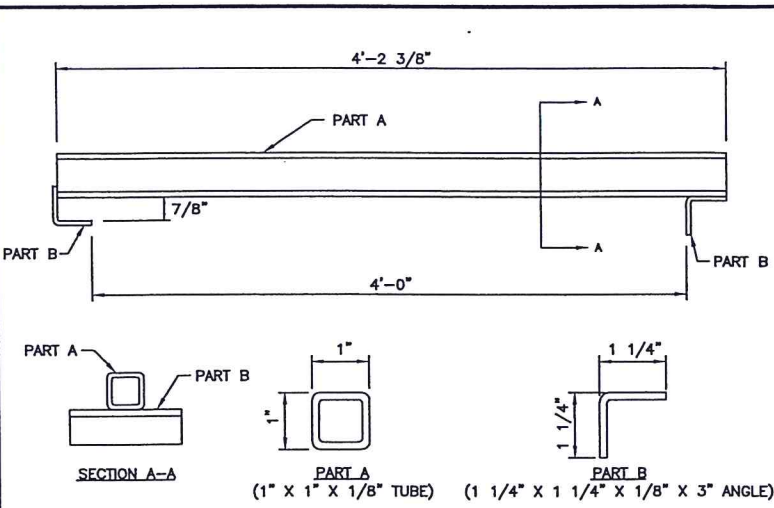
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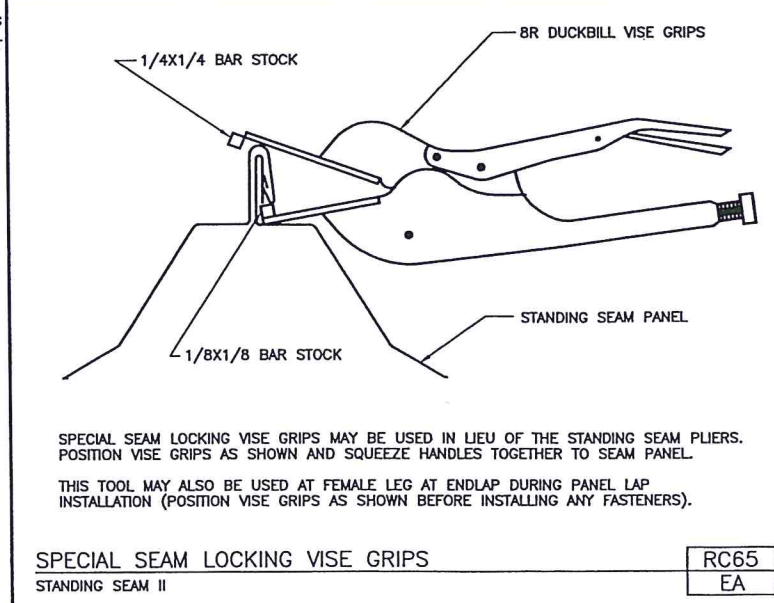
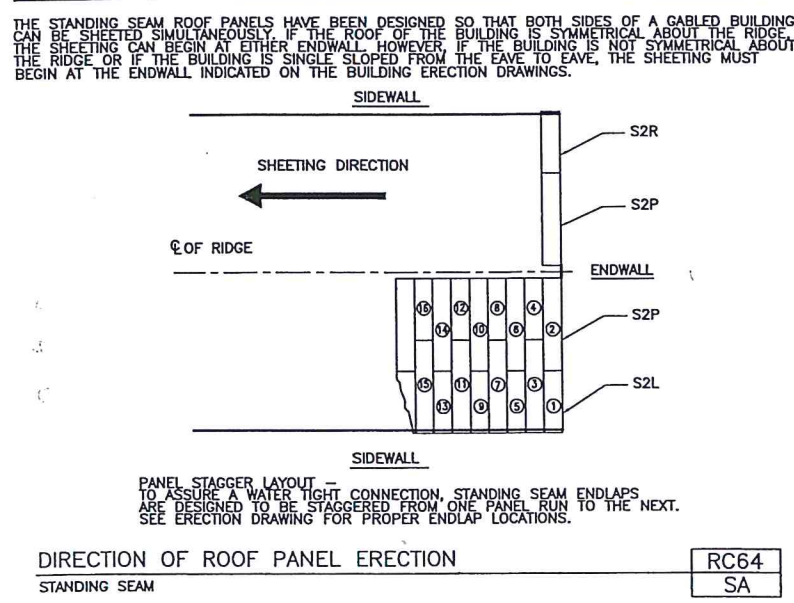
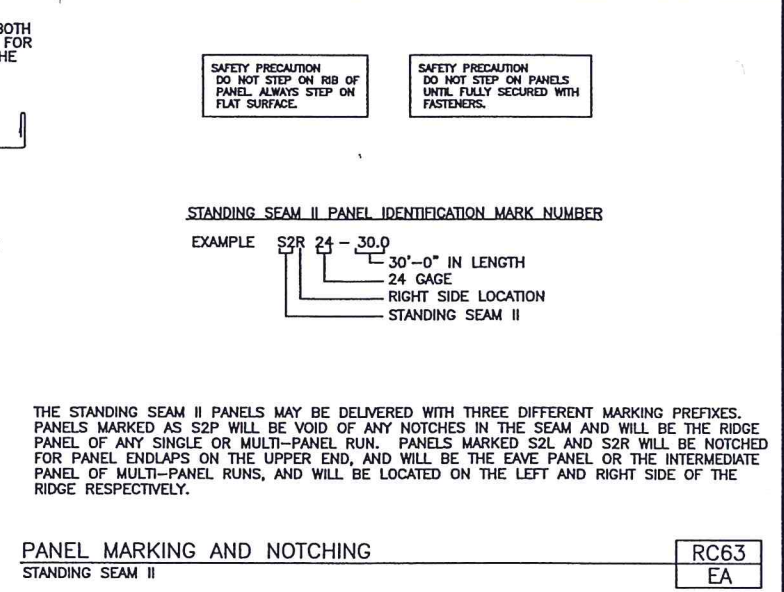
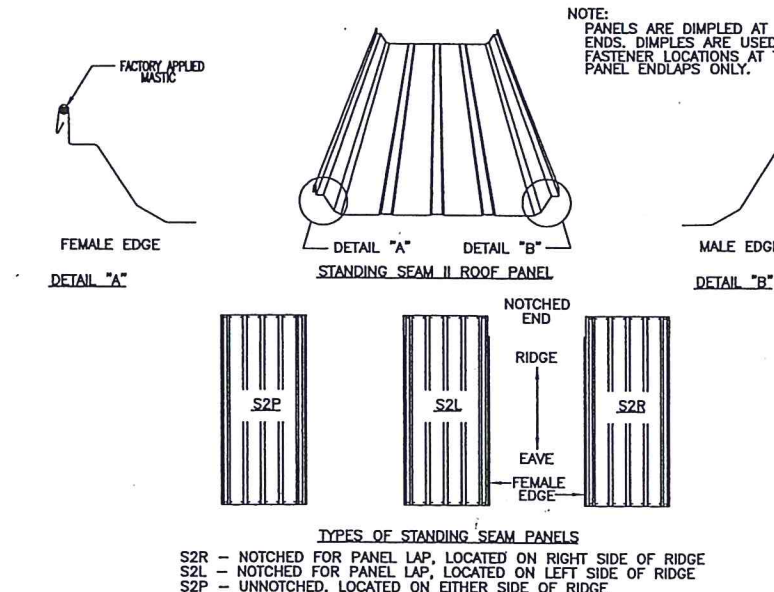
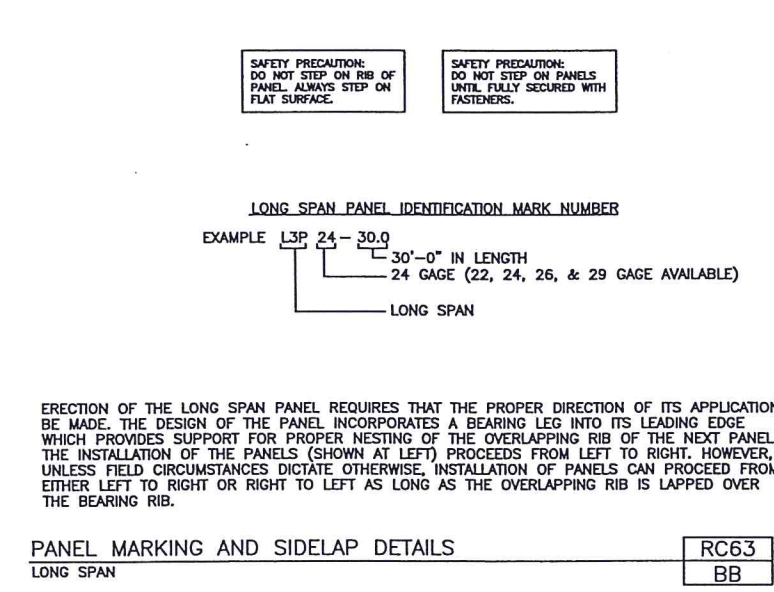
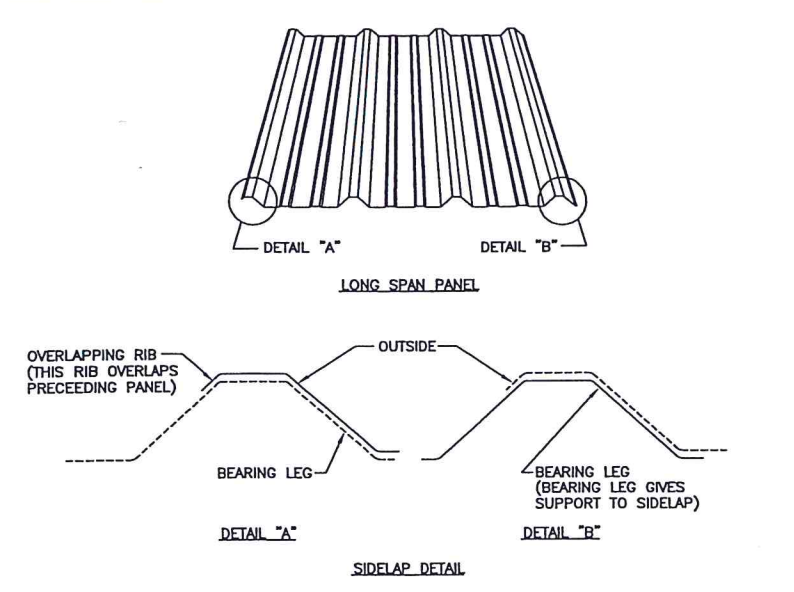
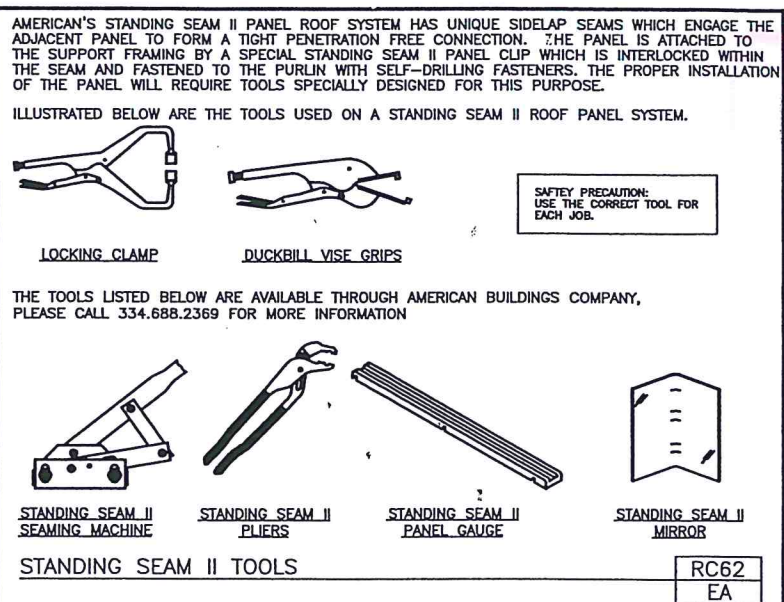
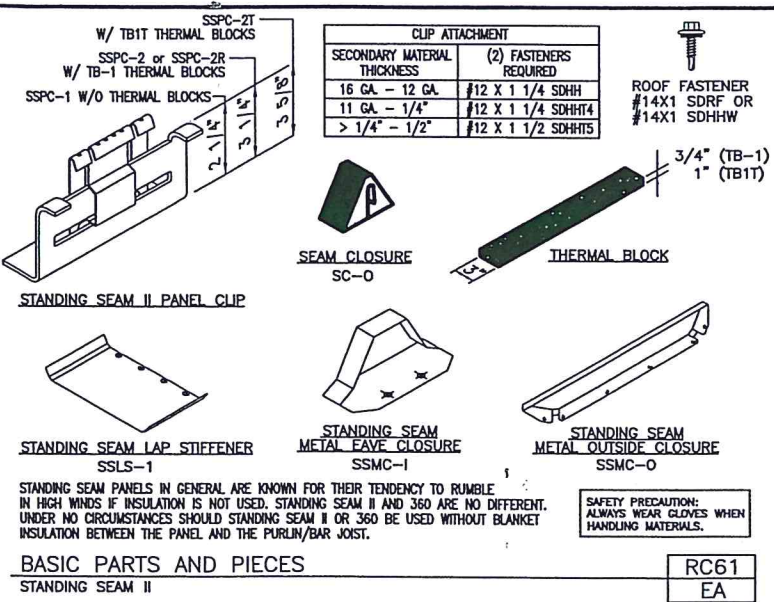
STANDING SEAM PANEL GAUGE LOCATIONS

RC42
SA



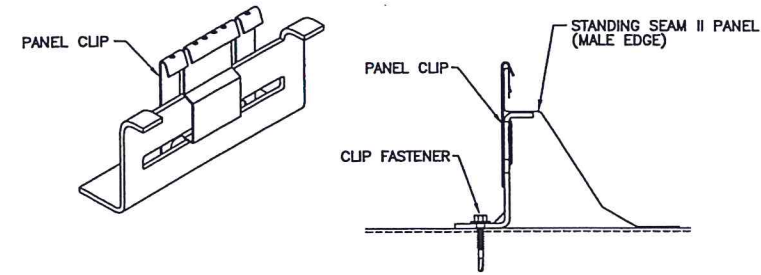
STANDING SEAM GAUGE BAR ASSEMBLY DETAIL

RC43
SA



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| | | | | | | | | | | | | | | [] FOR APPROVAL | | | | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | [] FOR PERMIT ONLY | | | | HORIZON CONSTRUCTION | | |
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THE STANDING SEAM II PANEL CLIP HAS 0.125"Ø HOLES IN THE TAB TO ALLOW FACTORY APPLIED CAULK IN PANEL TO SEEP THROUGH FORMING A WEATHERTIGHT SEAL.

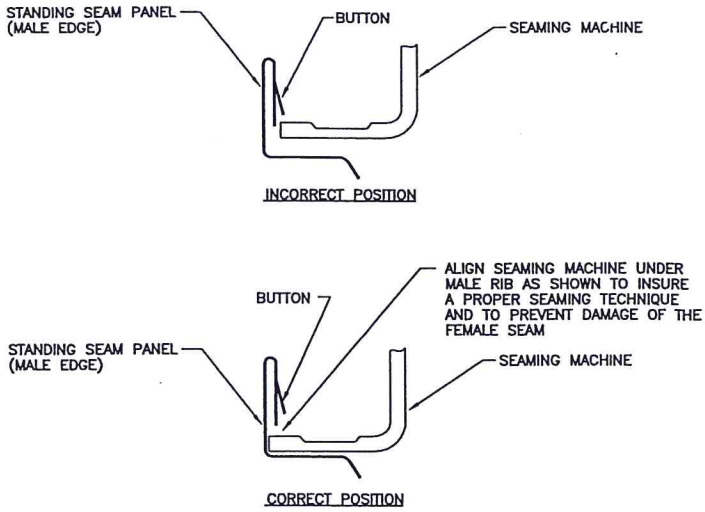


NOTE: BE CERTAIN THE SLIDING PORTION OF THE CLIP IS CENTERED ON CLIP BASE.

THE CLIP IS THEN INSTALLED OVER THE LEADING OR MALE EDGE OF THE PRECEEDING PANEL. MAKE SURE THAT THE CLIP TAB IS IN POSITION TO SUPPORT THE RIB. SECURE THE CLIP TO THE PURLIN WITH TWO STANDING SEAM CLIP FASTENERS.

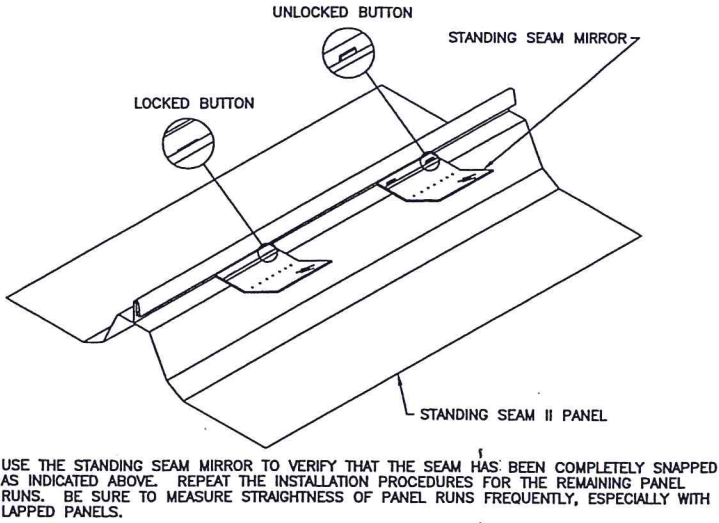
INSTALLATION OF PANEL CLIP
STANDING SEAM II

RC67
EA



STANDING SEAM SEAMING MACHINE
STANDING SEAM II

RC68
EA



STANDING SEAM MIRROR
STANDING SEAM II

RC69
EA

ERECTION NOTES:

1. ALL AREAS WHERE MASTIC IS TO BE APPLIED SHOULD BE WIPED CLEAN WITH A MILD DETERGENT OR AN ALL PURPOSE CLEANER BEFORE MASTIC APPLICATION. THIS WILL INSURE A GOOD SEALING SURFACE AND IMPROVE WEATHER TIGHTNESS.
2. THE BLANKET INSULATION MANUFACTURER RECOMMENDS THAT DOUBLE SIDED TAPE BE USED TO SECURE THE INSULATION TO THE EAVE. AMERICAN BUILDINGS COMPANY IS NOT RESPONSIBLE FOR THE INSTALLATION OR ATTACHMENT OF THE INSULATION. ADDITIONAL ATTACHMENTS (NOT BY ABC) MAY BE REQUIRED.
3. ALL EXPOSED FASTENERS SHOULD PENETRATE THE SEALANT FOR THE MOST WEATHER TIGHT CONNECTION IN ROOF AND WALL AREAS WHERE APPLICABLE.
4. WHEN FIELD CUTTING PANELS OR TRIM DO NOT USE ABRASIVE SAWS OR OTHER CUTTING METHODS WHICH PRODUCE HOT METAL PARTICLES OR BURN THE CUT EDGES. THESE METHODS WILL DAMAGE THE PAINTED AND GALVALUME FINISH AND VOID ANY WARRANTIES. USE DOUBLE CUT SHEARS, NIBBLERS OR OTHER CUTTING DEVICES WHICH DO NOT PRODUCE HOT METAL PARTICLES OR BURNED EDGES.

ERECTION NOTES
LONG SPAN

RC91
BA

ERECTION NOTES:

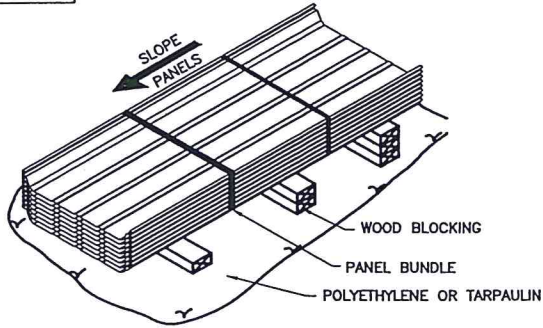
1. ALL AREAS WHERE MASTIC IS TO BE APPLIED SHOULD BE WIPED CLEAN WITH A MILD DETERGENT OR AN ALL PURPOSE CLEANER BEFORE MASTIC APPLICATION. THIS WILL INSURE A GOOD SEALING SURFACE AND IMPROVE WEATHER TIGHTNESS.
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3. DO NOT USE THE DIMPLES IN THE END OF THE PANELS TO LOCATE FASTENERS AT THE EAVE. DIMPLES ARE FOR THE FASTENERS AT THE PANEL ENDLAPS ONLY.
4. ALL EXPOSED FASTENERS SHOULD PENETRATE THE SEALANT FOR THE MOST WEATHER TIGHT CONNECTION.
5. WHEN FIELD CUTTING PANELS OR TRIM DO NOT USE ABRASIVE SAWS OR OTHER CUTTING METHODS WHICH PRODUCE HOT METAL PARTICLES OR BURN THE CUT EDGES. THESE METHODS WILL DAMAGE THE PAINTED AND GALVALUME FINISH AND VOID ANY WARRANTIES. USE DOUBLE CUT SHEARS, NIBBLERS OR OTHER CUTTING DEVICES WHICH DO NOT PRODUCE HOT METAL PARTICLES OR BURNED EDGES.

ERECTION NOTES – STANDING SEAM

RC91
SA

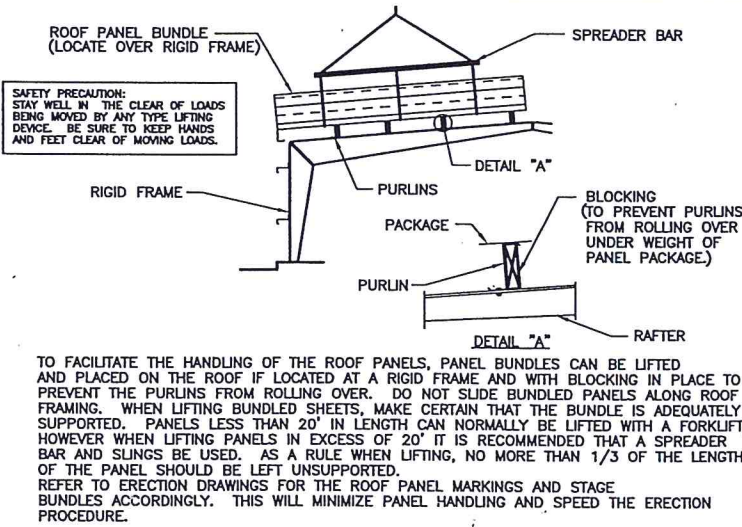
USE WOOD BLOCKING TO ELEVATE AND SLOPE THE PANELS IN A MANNER THAT WILL ALLOW MOISTURE TO DRAIN. WOOD BLOCKING PLACED BETWEEN PANEL BUNDLES WILL PROVIDE ADDITIONAL AIR CIRCULATION. COVER THE AREA BENEATH PANELS WITH POLYETHYLENE OR A TARPULIN TO PREVENT DIRT AND DEBRIS FROM ENTERING FEMALE SEAM.

SAFETY PRECAUTION
MAINTAIN A CLEAN AND ORDERLY WORK AREA.



PANEL STORAGE
STANDING SEAM

RC51
SA



PANEL STORAGE ON ROOF

RC54
AA

AMERICAN ROOF AND WALL PANELS

AMERICAN'S ROOF AND WALL PANELS INCLUDING COLOR COATED, ALUMINUM COATED AND GALVANIZED, PROVIDE EXCELLENT SERVICE UNDER WIDELY VARIED CONDITIONS. ALL UNLOADING AND ERECTION PERSONNEL SHOULD FULLY UNDERSTAND THAT THESE PANELS ARE QUALITY MERCHANDISE WHICH MERIT CAUTIOUS CARE IN HANDLING.

UNDER NO CIRCUMSTANCES SHOULD PANELS BE HANDLED ROUGHLY. PACKAGES OF SHEETS SHOULD BE LIFTED OFF THE TRUCK WITH EXTREME CARE TAKEN TO INSURE THAT NO DAMAGE OCCURS TO ENDS OF THE SHEETS OR TO SIDE RIBS. THE PACKAGES SHOULD BE STORED OFF THE GROUND SUFFICIENTLY HIGH ENOUGH TO ALLOW AIR CIRCULATION UNDERNEATH THE PACKAGES. THIS AVOIDS GROUND MOISTURE AND DETERS PEOPLE FROM WALKING ON THE PACKAGES. ONE END OF THE PACKAGE SHOULD ALWAYS BE ELEVATED ABOVE THE LOWER END TO ENCOURAGE DRAINAGE IN CASE OF RAIN.

ALL METAL PANELS ARE SUBJECT TO SOME DEGREE TO LOCALIZED DISCOLORATION OR STAIN WHEN WATER IS TRAPPED BETWEEN THEIR CLOSELY FITTED SURFACES. AMERICAN EXERCISES EXTREME CAUTION DURING FABRICATING AND SHIPPING OPERATIONS TO INSURE THAT ALL PANEL STOCK IS KEPT DRY. HOWEVER, DUE TO CLIMATIC CONDITIONS, WATER FORMED BY CONDENSATION OF HUMID AIR CAN BECOME TRAPPED BETWEEN STACKED SHEETS. WATER CAN ALSO BE TRAPPED BETWEEN THE STACKED SHEETS WHEN EXPOSED TO RAIN. THIS DISCOLORATION CAUSED BY TRAPPED MOISTURE IS OFTEN CALLED WET STORAGE STAIN.

THE STAIN IS USUALLY SUPERFICIAL AND HAS LITTLE EFFECT ON THE APPEARANCE OR SERVICE LIFE OF THE PANELS AS LONG AS IT IS NOT PERMITTED TO REMAIN ON THE PANELS. HOWEVER, MOISTURE IN CONTACT WITH THE SURFACE OF THE PANELS OVER AN EXTENDED PERIOD CAN SEVERELY ATTACK THEIR FINISH AND REDUCE THEIR EFFECTIVE SERVICE LIFE. THEREFORE, IT IS IMPERATIVE THAT ALL PANELS BE INSPECTED FOR MOISTURE UPON RECEIPT OF THE ORDER. IF MOISTURE IS PRESENT, DRY THE PANELS AT ONCE AND STORE IN A DRY, WARM PLACE, IF POSSIBLE.

WHEN HANDLING OR UNCRATING THE PANELS, LIFT, RATHER THAN SLIDE, THEM APART. BURNING EDGES MAY SCRATCH THE COATED SURFACES WHEN SHEETS ARE SLID OVER ONE ANOTHER. NEVER ALLOW PANELS TO BE WALKED ON WHILE ON THE GROUND.

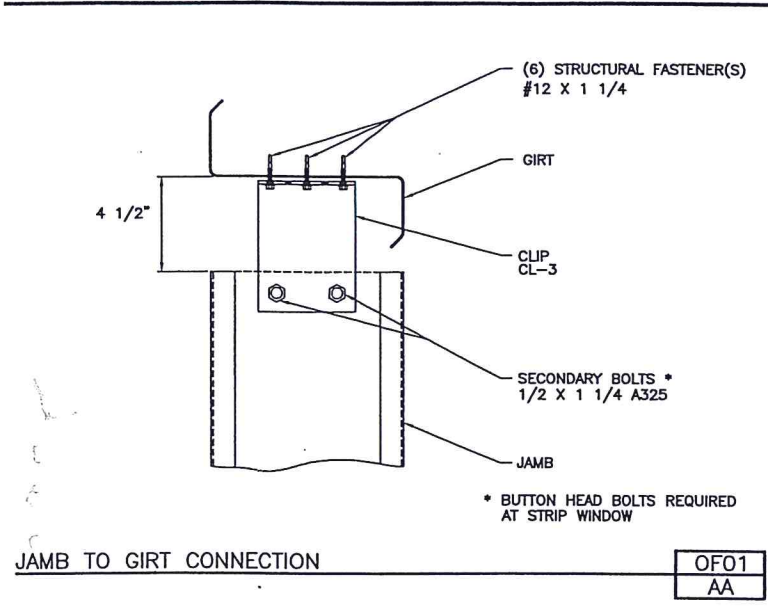
ROUGH AND IMPROPER HANDLING OF PANELS IS INEXCUSABLE AND A PRIME EXAMPLE OF POOR JOB SUPERVISION.

CAUTION: PANELS ARE SLIPPERY. OIL OR WAX THAT HAS BEEN USED ON THE ROOF AND WALL PANELS FOR PROTECTION AGAINST WEATHER DAMAGE WILL MAKE THEM A VERY SLIPPERY WALKING SURFACE. WIPE DRY ANY OIL THAT HAS PUDDLED FROM BUNDLES STORED ON A SLOPE. DEW, FROST OR OTHER FORMS OF MOISTURE GREATLY INCREASE THE SLIPPERINESS OF THE PANELS.

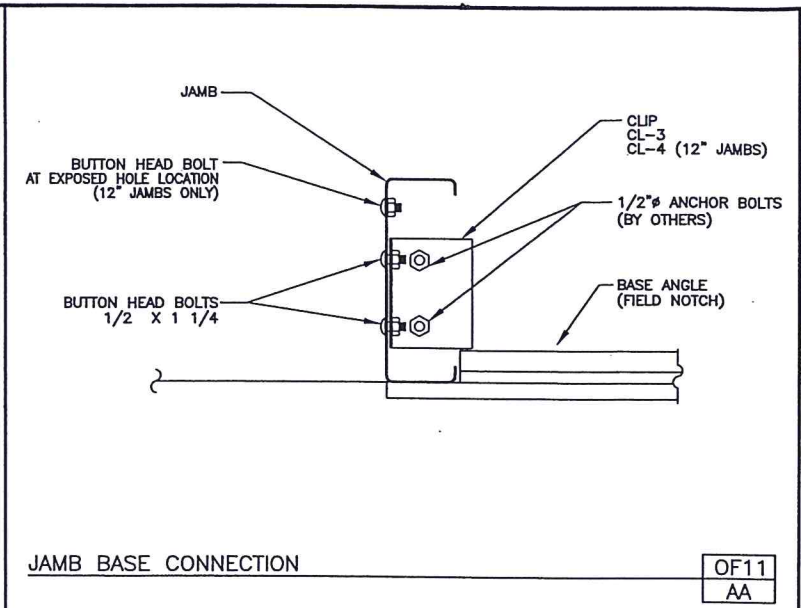
PANEL STORAGE NOTES

RC95
AA

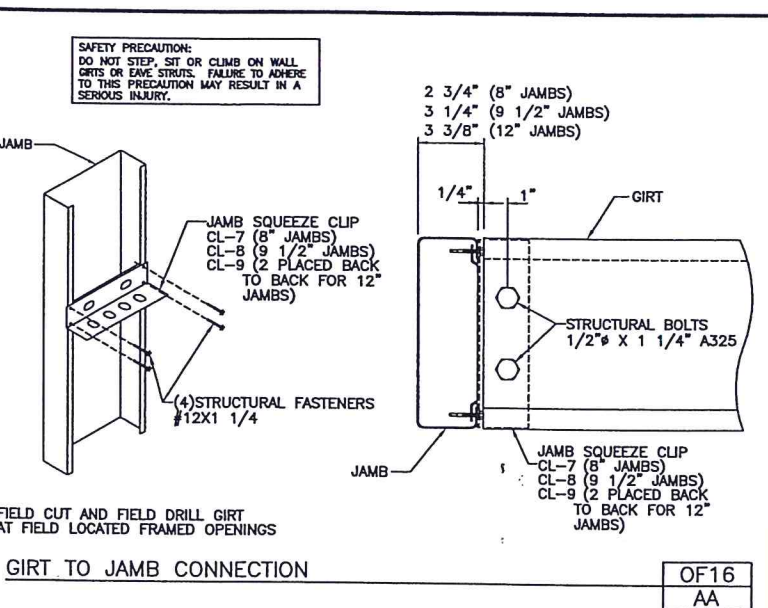
| NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | NO. | REVISIONS | MADE | CK'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | SCALE: | NONE | DATE | CONTRACT FURNISHINGS | EL PASO | ILLINOIS |
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| | | | | | | | | | | | | | | [*] FOR CONSTRUCTION [] FOR APPROVAL [] FOR PERMIT ONLY [] FOR PRELIMINARY | DRAWN BY: BJC | | | 3129 MAIN | | |
| | | | | | | | | | | | | | | CHECKED BY: | | | | KANSAS CITY, MO | | |
| | | | | | | | | | | | | | | DESIGN APPD BY: | | | | HORIZON CONSTRUCTION | | |
| | | | | | | | | | | | | | | CAD BY: | 08/28/08 | 12:28:37 | Ver. 30.1 | | | |
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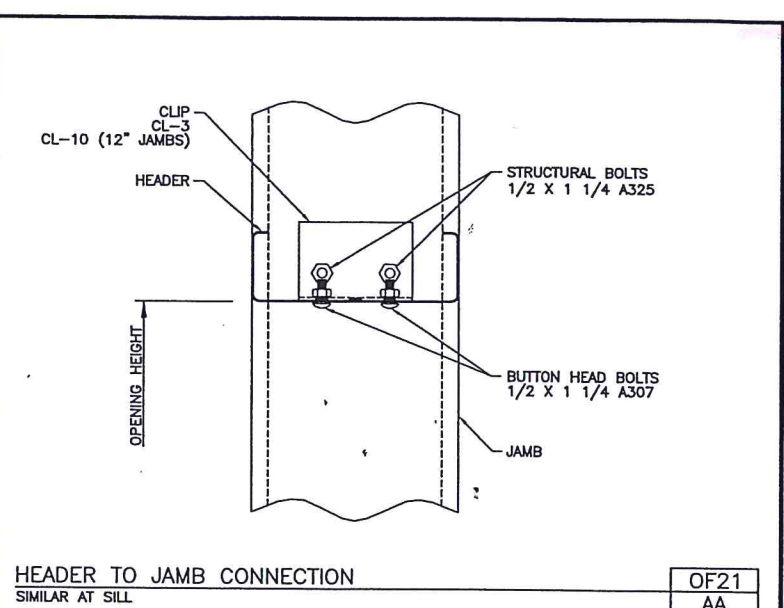
JAMB TO GIRT CONNECTION OF01 AA



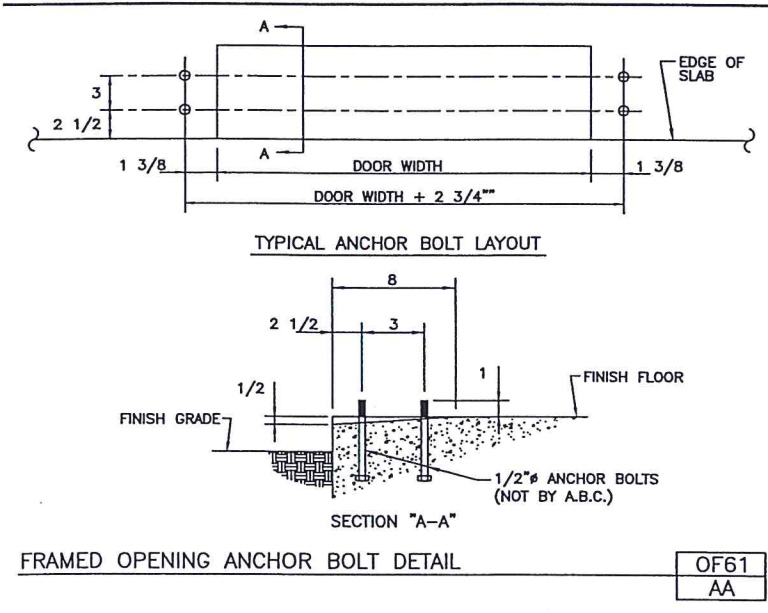
JAMB BASE CONNECTION OF11 AA



GIRT TO JAMB CONNECTION OF16 AA



HEADER TO JAMB CONNECTION OF21 AA



FRAMED OPENING ANCHOR BOLT DETAIL OF61 AA

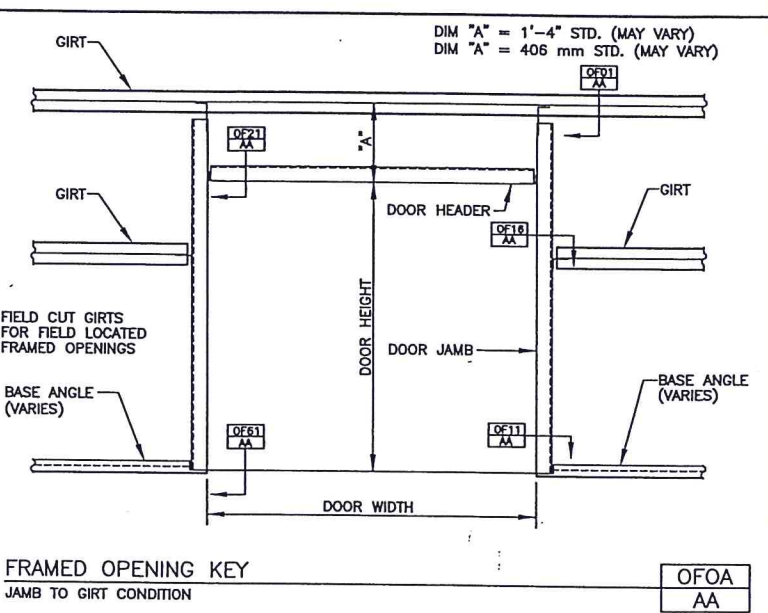
GENERAL NOTES FOR FIELD LOCATED FRAMED OPENINGS :

- OVERHEAD DOORS ARE FIELD LOCATED. CUT STANDARD PANELS AND GIRTS AS REQUIRED.
- MAXIMUM HEIGHT OF DOOR IS 2'-0" LESS THAN EAVE HEIGHT.
- ALL STRUCTURAL CONNECTIONS TO BE MADE AS SHOWN ON APPROPRIATE DETAILS.
- JAMBS MUST BE LOCATED A MINIMUM OF 1'-0" FROM CENTER LINE OF COLUMNS.

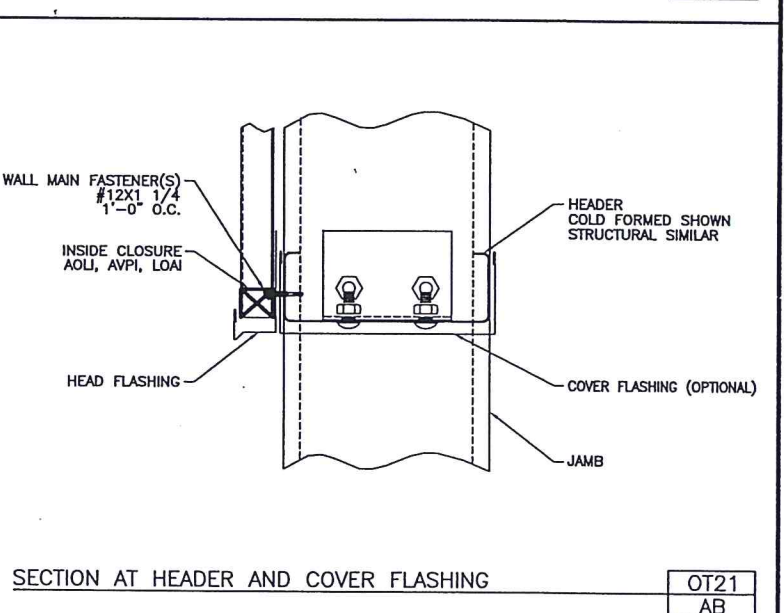
GENERAL NOTES FOR SHOP LOCATED FRAMED OPENINGS :

- OVERHEAD DOORS ARE SHOP LOCATED. SOME PANELS MAY REQUIRE FIELD CUTTING.
- ALL STRUCTURAL CONNECTIONS TO BE MADE AS SHOWN ON APPROPRIATE DETAILS.

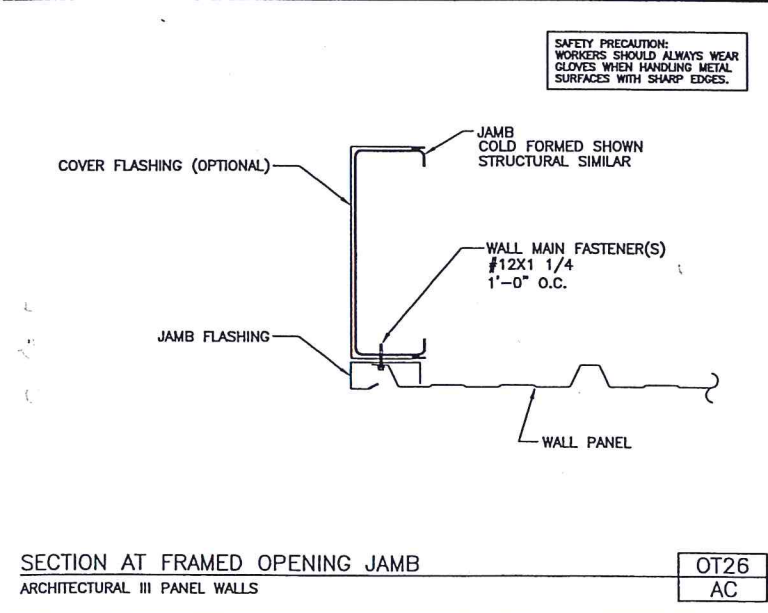
FIELD AND SHOP LOCATED FRAMED OPENING NOTES OF91 AA



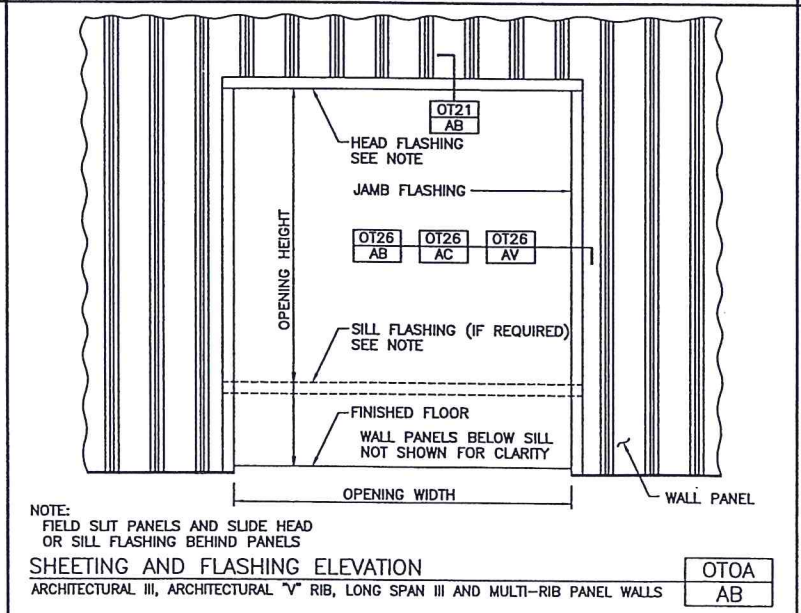
FRAMED OPENING KEY OF0A AA




SECTION AT HEADER AND COVER FLASHING OT21 AB



SECTION AT FRAMED OPENING JAMB OT26 AC



SHEETING AND FLASHING ELEVATION OTOA AB

| NO. | REVISIONS | | | | | MADE | CR'D | DATE | ENGR | DATE | NO. | REVISIONS | | | | | MADE | CR'D | DATE | ENGR | DATE | DRAWING SUBMITTAL STATUS | | | SCALE: | NONE | DATE | CONTRACT FURNISHINGS 3129 MAIN KANSAS CITY, MO HORIZON CONSTRUCTION | | | EL PASO |  | ILLINOIS | | | | | |
|-----|-----------|--|--|--|--|------|------|------|------|------|-----|-----------|--|--|--|--|------|------|------|------|------|--------------------------|--|----------------------|-----------|----------|-----------|--|--|--|----------------|---|----------|-----------------|--|--|--|--|
| | | | | | | | | | | | | | | | | | | | | | | | | [*] FOR CONSTRUCTION | DRAWN BY: | BJC | | | | | CHECKED BY: | | | DESIGN APPD BY: | | | | |
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